

**Round Hill / Uptown North Orange
Traditional Neighborhood Development
TND District Regulations
Code of Development**



Applicant:
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Round Hill Development
June 12, 2013
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Round Hill/UNO-TND District

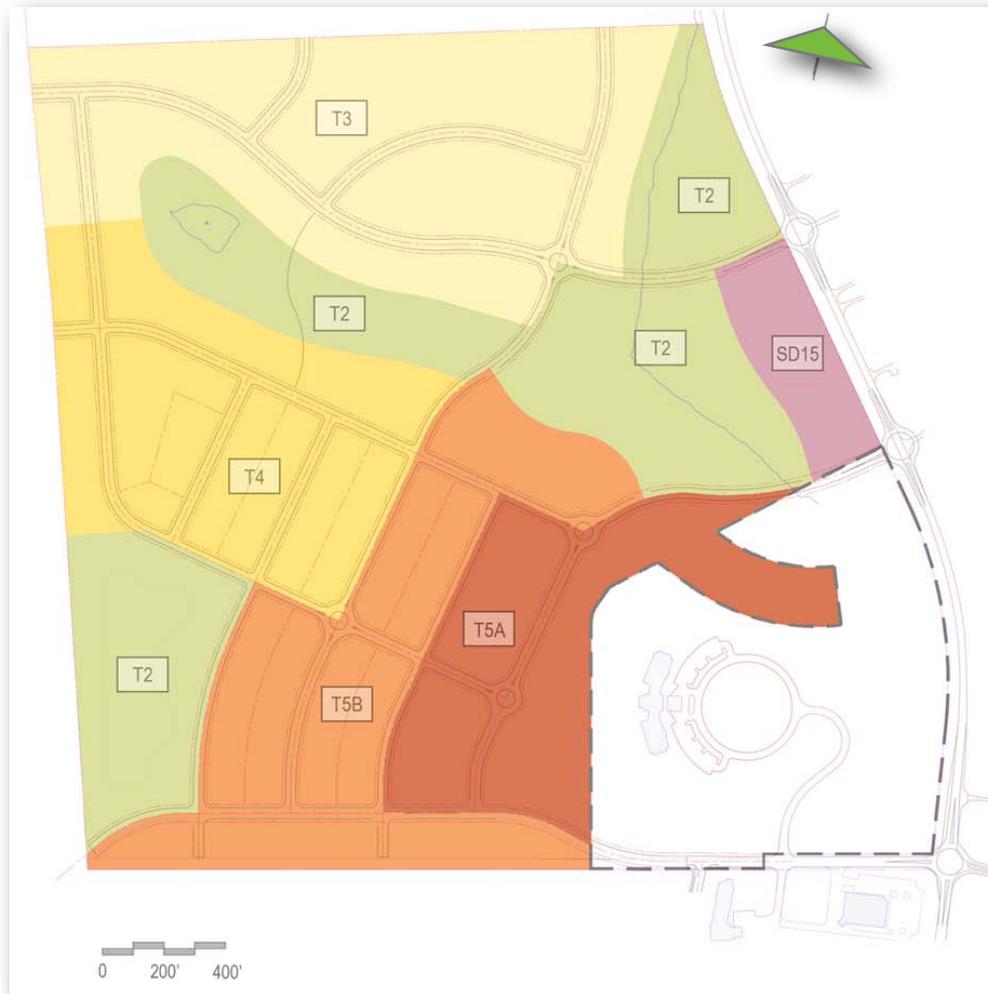
The Town of Orange

Round Hill / Uptown North Orange - Code of Development

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Section 1:
Purpose and Intent



Round Hill/UNO-TND District

The Town of Orange

Round Hill TND Code of Development

General Purpose and Intent

Round Hill / Uptown North Orange will be the Town’s first “traditional neighborhood designed” village to be approved under the Traditional Neighborhood Development (TND) District ordinance. This document establishes the regulatory Code of Development, as required by the Town’s TND District regulations for the 135-acre property. The mix, density, and location of land uses in Round Hill are regulated by six TND Transect Zones (T2, T3, T4, T5A, T5B, and SD15) that establish the physical character of the community.

These Transect Zones and other regulatory documentation contained herein provide the framework for the unified development of Round Hill’s residential neighborhoods, work places, streetscapes, civic areas and public parks. The Code of Development creates the framework that is intended to reach beyond traditional zoning approaches to focus on aesthetic and architectural principals. At its core, Round Hill seeks to promote a rich vision embracing a range of physical, social, and economic values. The overall goal is to promulgate a consistent and uniformly high level of quality throughout this new community.

By applying these TND Transect Zones to the largest undeveloped tract in the Town, the Owner/Developer seeks to carry out a vision for Round Hill that follows the path of the Town’s Comprehensive Plan supporting Traditional Neighborhood Design concepts and a range of uses and densities in the Uptown area. The TND Transect Zones provide the governing structure in support of this vision, along with zoning proffers proposed along with them. The Owner’s efforts over a multi-year period, as evidenced by many public work sessions, meetings, public hearings, and project revisions serve as the foundation for this set of regulatory structures for Round Hill.

The six TND Transect Zones community have been designed to support the ten goals of the Town’s TND District and represent a distinct neighborhood within the Round Hill. The transect zones define the scale, character, form and intensity of development to be allowed for individual buildings within each Round Hill transect. *The accompanying graphic exhibits depict the “vision” for the community and are included for illustrative purposes only.*

1) Compact Pedestrian-Oriented Neighborhoods

Fundamental to Traditional Neighborhood Development at Round Hill is the creation of compact neighborhoods oriented toward pedestrian activity. Round Hill incorporates a village commercial center, as well as residential areas, and a system of parks and open spaces, all organized within walking distance.

The Village Center transects (T5A and T5B) constitute Round Hill’s identifiable core, with retail and gathering spaces, eateries, offices, and destination activities that will be attractive to Round Hill residents and the community at large. The Village Center is

supported by residential areas, each featuring a variety of housing types, as well as its parks and gathering spaces of their own. The density of the village center and other residential transects is offset by large passive open space areas (T2 transects), as well as programmed parks and recreation areas within neighborhoods.



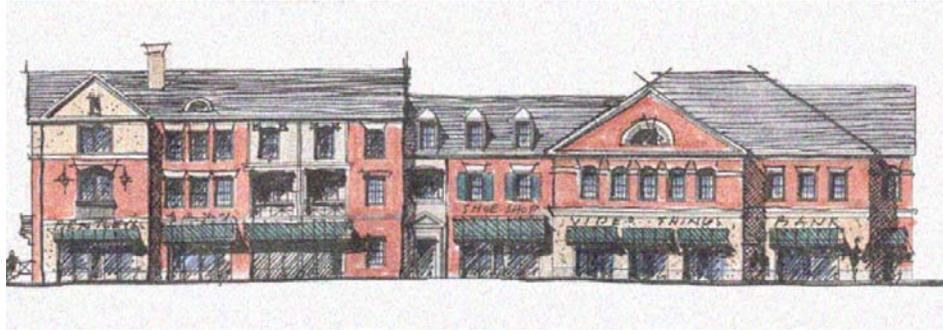
2) Housing Variety

Housing variety is an important goal within the Round Hill TND. While single family residences will cover the majority of the project's designed residential land area, ample space remains for other types of dwellings that may serve a broader socioeconomic spectrum. As a village-scaled neighborhood, the Transect Map introduces a mix of higher density land use options around the Village Center transects, including townhouses and condominiums. The critical mass of the residential uses will provide a unique variety and sense of vitality within the Village's internal blocks. In addition to conventional townhouse blocks, the Village Center will permit the development of upper level residences located in the same structures as non-residential uses.



3) Proximity of Residential, Commercial and Civic Uses

Round Hill will offer a blend of village-scaled residential, commercial, open space, and mixed uses, all interwoven within a comprehensive TND community. The community's density and connected grid of streets, sidewalks, and paths ensures that Round Hill's variety of residences, shops, workplaces, and civic uses are within walkable proximity.



4) Interconnected Streets and Blocks

The vehicular and pedestrian improvements within the project are designed around an interconnected grid and block system that is coordinated with extensive trails within open space areas. Responding to the Town's TND District goals for well planned neighborhood linkages, Round Hill incorporates a network of both vehicular accessways and pedestrian sidewalks and trails which will interconnect all village land uses. The Transect Map also provides for the appropriate locations for future connections to streets, sidewalks and pedestrian paths to the contiguous existing and future residential neighborhoods. These efforts all combine to offer multiple paths, and multiple modes, by which Round Hill and Town residents can access a variety of needs.



5) Transportation Hierarchy

The Round Hill TND intends to provide a convenient intra-neighborhood and inter-community transportation system, both for vehicular and pedestrian movements. Streets are designed to be scale appropriate for their intended uses and place within the overall community. This set of street section designs establishes a hierarchy of major streets at Round Hill entrances from Route 15 and Radney Road, to minor streets in interior residential areas to meet transportation needs while also providing calm and attractive streets for pedestrians.

Interconnectivity is also provided to ensure that the future development of adjoining properties can benefit from carefully placed access points. Pedestrian paths and sidewalks are incorporated for safe and efficient inter- and intra- neighborhood circulation. Special transportation features, including external and internal roundabouts, will create attractive and well controlled gateways into the village and within individual neighborhoods.

6) Natural Features and Open Space

The Round Hill TND goals for open space emphasize the need for outdoor recreational areas as well as passive open space for environmental protection. Thus, the project will include distinct park, recreation and open space areas within the thirty-five acres containing the T2 Transect Zones. This comprises 26% of the total TND property. Natural features include the preservation of an existing fishing pond and the development of a combined amenity/stormwater management pond near the Route 15 frontage.

These natural open spaces will provide an open space amenity around which paths and active park areas will be structured. The community sidewalk system will be connected to an interior path system within the T2 Transects to link residents, shoppers and visitors to open space and civic improvements that will be strategically designed and interspersed within Round Hill's activity areas and open spaces.



7) Neighborhood Squares, Greens, and Parks

In addition to passive open spaces and environmental areas, Round Hill will incorporate parks, squares, and landscaped streets into its TND Transect Zones. This will include locations for neighborhood gathering, aesthetic amenities, passive open space, and pedestrian facilities. Such parks, squares, and greens will be scaled appropriately to the buildings and uses that surround them, and will be designed in ways that are appropriate to their anticipated level and mode of use by Round Hill residents and visitors alike.



8) Community Identity and Focal Points

Round Hill's Village Center transects will be a major focal point for the TND community as well as for a larger portion of the Town's designated Urban Development Area. Major focal points will also be found in other districts, including parks, squares, and important civic and institutional uses incorporated into the important T2 open space district. Such landmarks will be set apart by their scale, architecture, and orientation to their surroundings, and function as community landmarks.



9) Scale, Form, and Character Compatibility

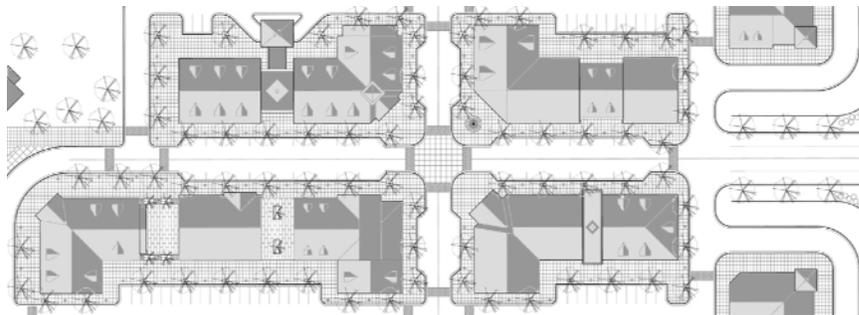
Round Hill's modest scale, from Village Center (T5 Transects) to residential neighborhoods, will promote a true sense of place, and integrate this new community within the Town rather than stand out. The entirety of the community will be built in such a way as to ensure the compatibility of architectural scale, massing, height, orientation, and landscaping among new buildings within Round Hill, as well as with existing Town and Virginia architecture to create inviting streetscapes and neighborhood center environments in which pedestrians will feel at home living, walking, playing, and shopping.

The architectural style and quality of Round Hill will be self-governing, with a Design Review Committee established to ensure that each proposed building and landscape meets not only the Town's standards, but also the strenuous test of the Owner's vision.



10) Public Street Edge and Orientation

The goals for the Town's TND District recognize that streetscapes are the fundamental element of public space within the neighborhood. To ensure that Round Hill is inviting to all, its public streets and other internal streets will be modeled after the scale and capacity of those found in traditional Virginia towns and villages, with street trees, quality street lighting, benches, and specialty signage. Public and private buildings will form a consistent, distinct edge, and will be oriented toward streets to define the border between public street spaces and the private lot and block interiors.



Section 2:
Definitions



Round Hill/UNO-TND District

The Town of Orange

Definitions

The following definitions are meant to further explain certain terms used in this Code of Development. The definitions contained here are not meant to replace or override any definition given in the Town of Orange Zoning Code. In the case of any dispute between these definitions and those in the Zoning Code, the Zoning Code definition shall be held to apply.

Accessory Structure: A detached, non-primary structure on a lot that may include a garage, studio, apartment, or other use that is subordinate in size and character to the principal, primary building situated on the same lot. Accessory Structures shall not be located in the front yard.

Accessory Unit: A secondary living unit on a residential lot, which can be rented out as an additional residential unit or used as a home occupation space by the owner of the primary principal structure. It can be located in a primary building attic or basement or over a garage in an accessory/ancillary building on the same lot as the main structure.

Alley: A low-capacity vehicle travelway, with two-way yield operation along the rear of properties in a private access easement. It not only provides access to the rear of buildings, but also vehicle parking (e.g. garages), and locations for certain service functions.

Anchor Establishment: A commercial or recreation-oriented, single-use establishment that is typically located in a shopping center, enclosed mall, free standing pad site, or urban main street area which provides a destination attraction for a regional or sub-regional market segment. Examples include, but are not limited to, large footprint clothing and department stores, grocery stores, physical fitness facilities, theaters and cinemas. The anchor establishment serves as a catalyst to enhance the local marketplace support for smaller scale retail and nonresidential uses.

Apartment: A portion of a building consisting of a single dwelling unit within a multi-family building. Apartments may be located in buildings containing more units of the same use or in buildings containing a mix of uses.

Arcade (or Colonnade): A building frontage where the façade is an arched colonnade that partially encloses pedestrian or café space on the ground level.

Average Daily Traffic (ADT): The average total number of vehicles that traverse a road on a typical day.

Awning: A cantilevered, projected or suspended cover over the sidewalk portion of the street-space. It is usually made of canvas or metal, is often adjustable and placed over the sidewalk, windows, or doors to provide protection from sun and rain.

Balcony: An exterior platform attached to the building façade. Balconies aligned vertically on adjacent floors may post up to one another and share a single roof element.

Bio-retention: A water quality practice that uses landscaping and soils to treat urban storm water runoff by collecting it in shallow depressions before filtering it through a fabricated planting soil media.

Block: The aggregate of private lots, passages, rear lanes and alleys, circumscribed by public streets. Blocks are measured at the frontage lot lines (along required right of way).

Block Corner: The outside corner of a block at the intersection of two public streets. Some building and street furniture requirements are specific to block corners or corner lots.

Buildable Area: The area of the lot that building(s) may occupy, which includes the entire area of the lot within the minimum setbacks.

Building Height: The vertical distance measured in stories or feet from the finish grade of the sidewalk at the front property line, to the eave of the roof, or bottom of parapet wall.

Civic Building or Space: A building or space intended for public assembly, governmental or non-governmental institutional use or some other civic function. Civic buildings can include but not be

limited to municipal buildings, churches, libraries, schools, daycare, educational, cultural and recreational facilities. Civic buildings may be allowed in the T2 transect as a special use.

Common Lot Lines: Lot lines shared by adjacent private lots.

Conservation Area: Large open spaces made up of the T2 transect, available for recreation and other civic use that may also function as a part of an overall system of storm-water management (including rain gardens) or wetlands preservation. Such areas may be contiguous with other greenways and natural corridors in the region and may be linear in that they follow natural stream networks. Paths and trails shall be provided within conservation areas, for the purposes of connecting streets and neighborhoods, as well as for active recreation.

Cottage Lot: The smallest of the allowable single family detached residential lot types.

Design Review Committee: The administrative vehicle of the Round Hill / Uptown North Orange development tasked with ensuring that submissions for individual building projects at the lot level adhere to project guidelines for architectural quality, landscape design, and other parameters prescribed in this and other documents. The Design Review Committee does not perform a zoning function, review site plans, or otherwise assist or interfere with the planning review function of the Town of Orange.

Developer: The person or entity charged with planning and implementing all or part of the proposed Round Hill / Uptown North Orange development. The Developer and Owner may be one and the same.

Duplex: A structure with two dwelling units placed one beside the other, sharing a common wall.

Encroachment: Building elements allowed to extend into the setback area, such as balconies, bay windows, porches, chimneys, and roof overhangs, typically on all sides of the building.

Estate Lot: The largest of the allowable single family detached residential lot types. Appropriate only in the T3 transect.

Façade: Building face; the building elevation facing the public street space. Building walls facing interior courts, common lot lines, and alleys are not facades.

Facade Build-out along Frontage: The extent to which the lot width along the street frontage is occupied by a facade, measured as a % of the frontage width. The percent of lot width occupied by the façade must fall within a regulated range established in each transect.

Fenestration: Openings in the building wall allowing light, views, and entrance/exit between interior and exterior. Fenestration includes both windows and doors.

Floor Area Ratio: The ratio of the total floor area (for all floors) of one or more buildings to the overall land area of the lot, parcel, or transect that the building occupies.

Frontage or Frontage Line: The front or side property line and/or right of way line of a building lot (equal to lot width), measured along a public street. Alleys are not considered to have frontages.

Frontage Types: Additional elements of a building's construction, located between the building façade and public street frontage, including, but not limited to, porches, stoops, arcades, and awnings. Allowable frontage types, as well as specific dimensional standards, are included in the individual transect district regulations.

Ground Story or Floor: The first floor of a building that is directly accessible to the public street, either at grade or elevated via ramps or steps. The next Story above the Ground Story is the second floor. All ground stories serving the public should be ADA compliant.

Guideline: A table, graphic, or piece of text that sets forth a desired or preferred condition or outcome but whose actual application is up to the discretion of the user (ie. developer.) Guidelines are not mandatory but intended to be a useful reference.

House Lot: The second smallest of the allowable single family detached residential lot types.

Illustration or Illustrative: A graphic or image intended to illustrate the application of a standard or principle, but is not in and of itself mandatory.

Impervious Cover: Any surface in the urban landscape that cannot effectively absorb or infiltrate rainfall.

Live-Work Building: A type of vertically mixed use building that consists of a single dwelling unit located above a single ground floor commercial use, with the entire structure under single ownership.

Lodging: Premises available for daily or weekly renting of bedrooms.

Lot Area: The total area within a lot boundary.

Lot Coverage: The extent to which a lot is built out, inclusive of primary and ancillary buildings (but not excluding any open area requirements) measured as a % of the lot area. Maximum % lot coverage sets a maximum criterion for building mass and scale.

Minimum Lot Size: The minimum area of an individual lot within a particular zoning category.

Mixed-Use: A single structure or parcel containing a variety of different land uses, such as residential, commercial, or civic uses.

Mixed-Use Street: A street within any transect that contains a mix of uses which may include commercial, residential, office, and civic uses within close proximity.

Multi-Family Dwelling: A building or portion of a building that consists of two or more dwelling units stacked on top of each other. Also known as an apartment building.

Neighborhood: A Neighborhood is an area of residential, commercial, or mixed use development within a reasonable walking distance. Neighborhoods should be designed to have both a discernable center and edges.

Open Space: Land that is set aside for outdoor enjoyment and/or natural preservation.

Parking Entry: An opening (with curb cut) in the building façade and/or street wall where vehicles may enter into the block interior for general parking and business servicing.

Parking Impact Study: Upon consideration of a Parking Impact Study, the Zoning Administrator may allow exceptions to Town off-street parking requirements for a particular use.

The study shall contain a detailed assessment of parking needs, shared parking options, and on-street parking availability. The scope and specific requirements of the study shall be mutually agreed upon by the property owner and Zoning Administrator based on the size and uses of the proposed development, and shall be prepared by a competent and experienced professional.

Parking, Off-street: Driveways or parking lots within the boundaries of a property.

Parking, On-street: Parallel parking typical along public streets as seen in traditional towns and villages. This parking may be cited in individual parking impact studies to satisfy local parking requirements for all uses.

Parking Ratio: An expression of the required parking spaces that must be provided for a particular land use, often stated as a ratio of x spaces per y units in residential calculations or x spaces per 1000 square feet in non-residential calculations.

Primary Frontage: The side of a building entrance to a structure which is located along a public street. For corner lots, or where a lot fronts more than one public street, the primary frontage is that frontage along a major village street or Radney Road, as established by the Regulating Plans, or along that street with the highest expected traffic. The main entrance to a building or use should be along the primary frontage.

Property Line: The boundary line of a lot that demarcates ownership rights and responsibilities.

Regulating Plan: The set of plans or maps of the overall development that depict where spatial regulations, including transects, street types, open spaces and utilities, apply. The regulating plan is a guide for site planning of individual building and development projects, and may change over time as such development projects are approved by the Town.

Retail: Any use associated with the sale of merchandise or food service.

Right of Way (ROW): The designed area of a street that is dedicated to public use and ownership, including travel lanes, parallel parking, landscape areas, and sidewalks. See individual street section designs.

Setback: Minimum distance between the building face (i.e. façade) and property (i.e. lot boundary) line. The setback may be encroached upon by certain building elements, including but not limited to porches, stoops, chimneys, balconies, awning, and bay windows.

Shared Parking: Two or more uses such as office and residential that have peak parking loads at different times of the day or week, can often share their parking with each other and take advantage of the underutilized parking at one of the facilities. This reduces the total amount of parking thereby allowing higher densities and reducing impervious cover.

Single Family Detached (SFD): One dwelling unit on its own lot, detached from other adjoining lots.

Stoop: A small platform and/or entrance stairway at a house door. Stoops may be roofed, but are not enclosed.

Story Height: That space within a building and above grade, which is situated between one floor level and the floor level next above, or if there is no floor above, the ceiling or roof above.

Street: A roadway designed to provide direct local access to abutting land. Streets within Round Hill / Uptown North Orange are governed by the Thoroughfare Map Diagram and related Street Section Designs.

Street Light: A luminaire installed on both sides of streets, along the street tree alignment line at regular intervals.

Street Tree: Trees planted in rows parallel to public streets, within the prescribed planting zone at regular intervals to provide visual interest and shade to pedestrians.

Swale: An open drainage channel or depression explicitly designed to detain and promote the filtration of storm water runoff.

Townhouse: One dwelling unit attached by a common wall to another dwelling unit, or to several units in a row. A Townhouse is generally a fee simple unit, from ground to roof, with no units above or below.

Transect: A cross-section of a Neighborhood divided into zones containing a mix of activities and reflecting a continuum of intensity of uses, and used to identify the level of urban character and intensity of a development project.

Vertical Mixed Use: A single structure containing multiple, different land uses arranged vertically. For example, upper floors may be used for office, residential, or lodging, while the ground floor may contain retail or office uses.

Villa Lot: The second largest of the allowable single family detached residential lot types. Appropriate only in the T3 transect.

Yard: The space in front, behind or to the side of a building between the prescribed setback line and lot line.

Section 3:
Permitted Uses



Round Hill/UNO-TND District

The Town of Orange

Permitted Uses: Round Hill / UNO TND Districts

The following table lists the uses permitted by-right, uses requiring a special use permit, and prohibited uses in each of the transects as designated with the zoning of the Round Hill / UNO TND District. Table 3.1 replaces the land use table contained in the original UNO TND zoning application. Land uses that are not specifically designated in the table as approved for By-Right (B) or Special Use (S) are prohibited.

Table 3.1:
Table of By-Right and Special Permit Transect Uses – Round Hill TND

B = By-Right Use
S = Special Use
Blank = Prohibited

Round Hill Transect Zones

Permitted Uses	T2	T3	T4	T5-A	T5-B	SD-15
Accessory dwelling units		B	B	B	B	
Accessory use or structure (non-dwelling)		B	B	B	B	B
Adult-oriented businesses						
Agriculture	B	B	B	B	B	B
Amusement arcade or game rooms				B	B	B
Animal hospitals or clinics				S	S	B
Animal kennels & grooming				S	S	B
Antique stores			S	B	B	B
Art studios and galleries			S	B	B	B
Asphalt plants						
Auctions				B	B	B
Automotive body repair shops				S	S	B
Automobile dealerships				S	S	B
Banks and financial institutions			S	B	B	B
Barbershops				B	B	B
Beauty parlors and nail salons			S	B	B	B
Bed and breakfast		S	S	B	B	B
Boarding houses			S	S	S	
Breweries				B	B	B
Bulk fuel storage and distribution systems						
Car washes				S	S	B
Child care centers	S	S	S	B	B	B

Permitted Uses	T2	T3	T4	T5-A	T5-B	SD-15
Churches	B	B	B	B	B	B
Clubs				B	B	B
Cluster developments		B	B	B	B	
Communication and broadcasting studios				B	B	B
Communication antennae or devices on existing structures	S	S	S	S	S	S
Communication towers (new) fewer than two-hundred (200) feet in height	S	S	S	S	S	S
Concrete plants						
Conference centers				B	B	B
Convenience stores			S	B	B	B
Country inns		S	S	B	B	B
Crematoriums				S	S	S
Crop growing and storage	B	B	B	B	B	B
Data centers				B	B	B
Drive-in facilities				S	S	B
Drive-through facilities				S	S	B
Duplex dwellings		B	B	B	B	B
Electrical substation	S	S	S	S	S	S
Family day care (≤ 5 children)		S	S	B	B	
Family day care (> 5 children)			S	B	B	
Farm and garden supply stores				B	B	B
Farmers market	S			B	B	B
Fire and rescue squad stations	S	S	S	B	B	B
Foundries						
Funeral homes				B	B	B
Furniture and cabinet-making shops				B	B	B
Golf courses	S	S	S	S	S	S
Government buildings	S		S	B	B	B
Ground floor residential		B	B	B	B	
Group homes		B	B	B	B	
Health and fitness centers			S	B	B	B
Heavy manufacturing						
Heliports				S	S	S
Home occupations (≤ 2 nonresident employees)		B	S	S	S	
Home occupations (> 2 nonresident employees)		S	S	S	S	S
Hospitals				B	B	B
Hotels				B	B	B

Permitted Uses	T2	T3	T4	T5-A	T5-B	SD-15
Indoor and outdoor theaters				B	B	B
Indoor recreation facilities	S		S	B	B	B
Junk or salvage yard						
Laundry, laundromats, dry cleaners				B	B	B
Libraries	S			B	B	B
Light manufacturing					S	S
Live-work units			B	B	B	
Machine or equipment sales, service and/or rental				S	B	B
Manufactured homes (new)						
Manufactured homes (replacement of existing only)						
Medical clinics and offices			S	B	B	B
Mini-warehouses and self-service storage units				S	S	B
Mixed commercial use dwellings (i.e. mixed-use)				B	B	B
Monument manufacturing and sales				S	S	B
Motels				B	B	B
Multi-family dwellings		S	B	B	B	S
Museums			S	B	B	B
Nursing homes and assisted living facilities			S	B	B	S
Outdoor recreation facilities	B	B	B	B	B	B
Overhead utilities, wires and transformers	S	S	S	S	S	S
Parking garage			B	B	B	B
Parks and playgrounds	S	B	B	B	B	B
Pet stores				B	B	B
Pharmacies				B	B	B
Professional offices			B	B	B	B
Public animal kennels				S	S	B
Public transportation facilities	S	S	S	B	B	B
Recycling centers				S	S	S
Recycling collection points	S	S	B	S	B	B
Repair garage				S	S	B
Repair garage, commercial vehicle				S	S	B
Restaurants			S	B	B	B
Retail and wholesale bakeries			S	B	B	B
Retail and wholesale			S	B	B	B

Permitted Uses	T2	T3	T4	T5-A	T5-B	SD-15
establishments						
Retail and wholesale printing and photo processing				B	B	B
Retail furniture and appliance stores				B	B	B
Retail nurseries and greenhouses				B	B	B
Retail upholstery shops				B	B	B
Schools	S	S	S	B	B	B
Service and gas stations				S	S	S
Shopping centers				B	B	B
Single-family dwellings		B	B	B	B	
Spas			S	B	B	B
Streets and alleys	B	B	B	B	B	B
Supermarkets and grocery stores			S	B	B	B
Tattoo shops				S	S	S
Technology services			S	B	B	B
Textile mills						
Thrift or secondhand store			S	B	B	B
Townhouses		B	B	B	B	S
Tree farms, arboretums	S				B	
Truck terminals						
Utilities	S	S	S	S	S	S
Warehouses						S
Water storage tanks	S	S	S	S	S	S
Wineries, orchards, and vineyards	S			B	B	B
Yard or garage sales	B	B	B	B	B	B
SUP shall be required for non-residential uses with first floor footprints that exceed the gross square footage as specified in the individual transect zone		S	S	S	S	S

Section 4:
Regulating Plans

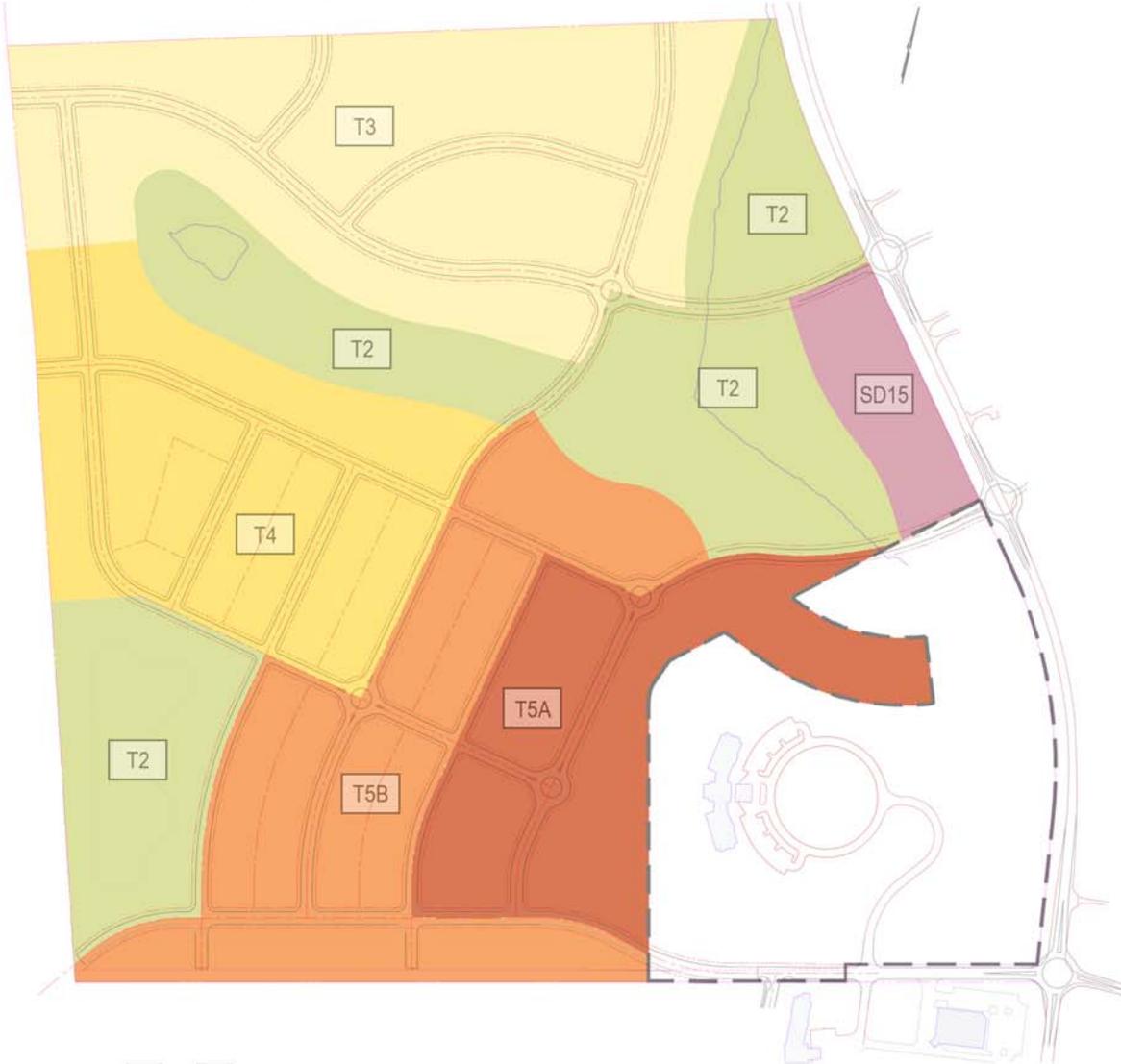


Round Hill/UNO-TND District

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Figure 4.1 Transect Map Diagram

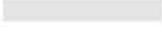


<u>ZONE</u>	<u>APPROXIMATE GROSS ACREAGE</u>
existing	
--- area not to be rezoned	26.77 acres
area to be rezoned	
SD15 special district SD15	4.40 acres
T5A center zone T5A	17.06 acres
T5B center zone T5B	23.83 acres
T4 general zone T4	22.87 acres
T3 edge zone T3	31.93 acres
T2 edge zone T2	34.94 acres
total area:	135.03 acres

Round Hill Code of Development

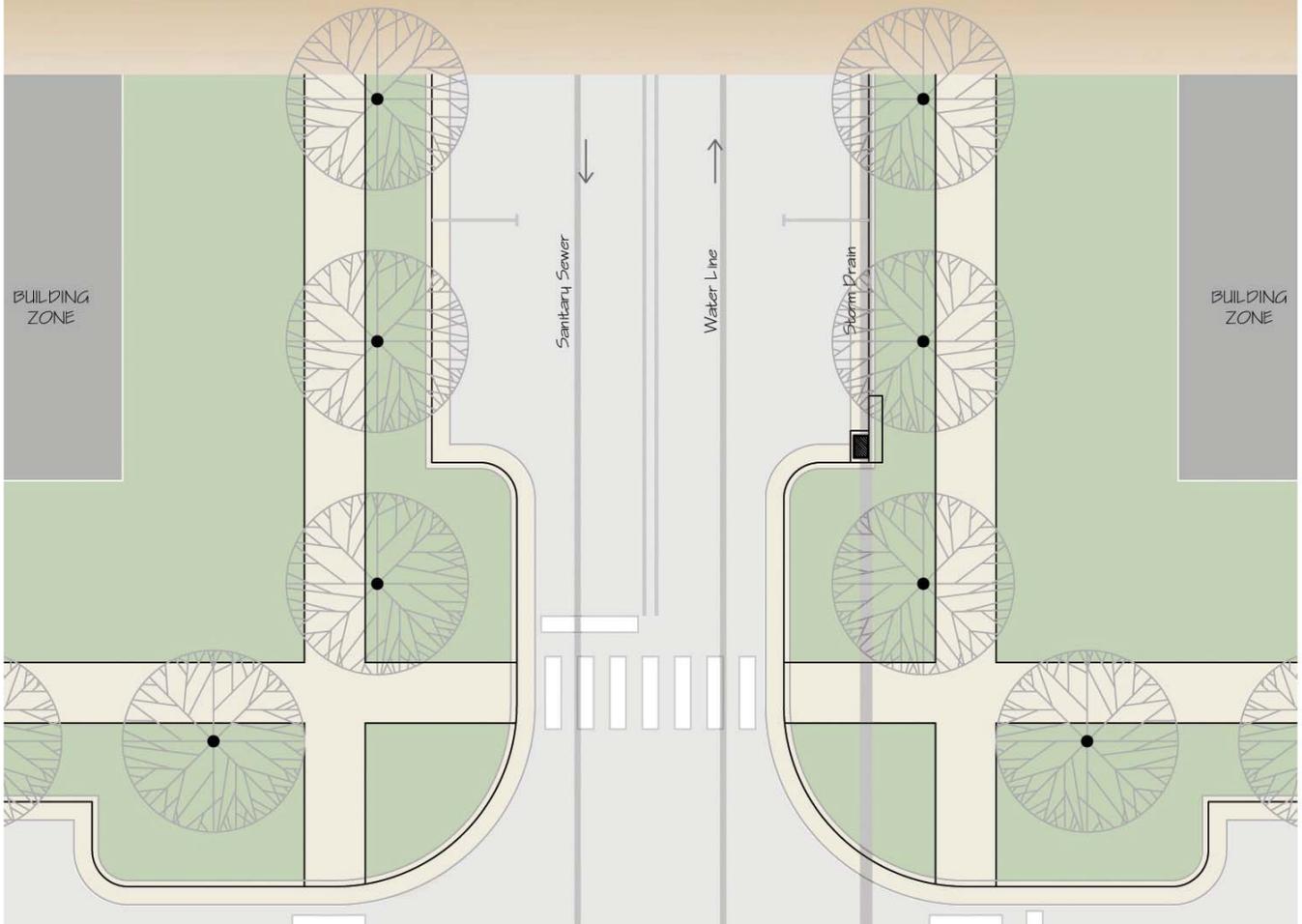
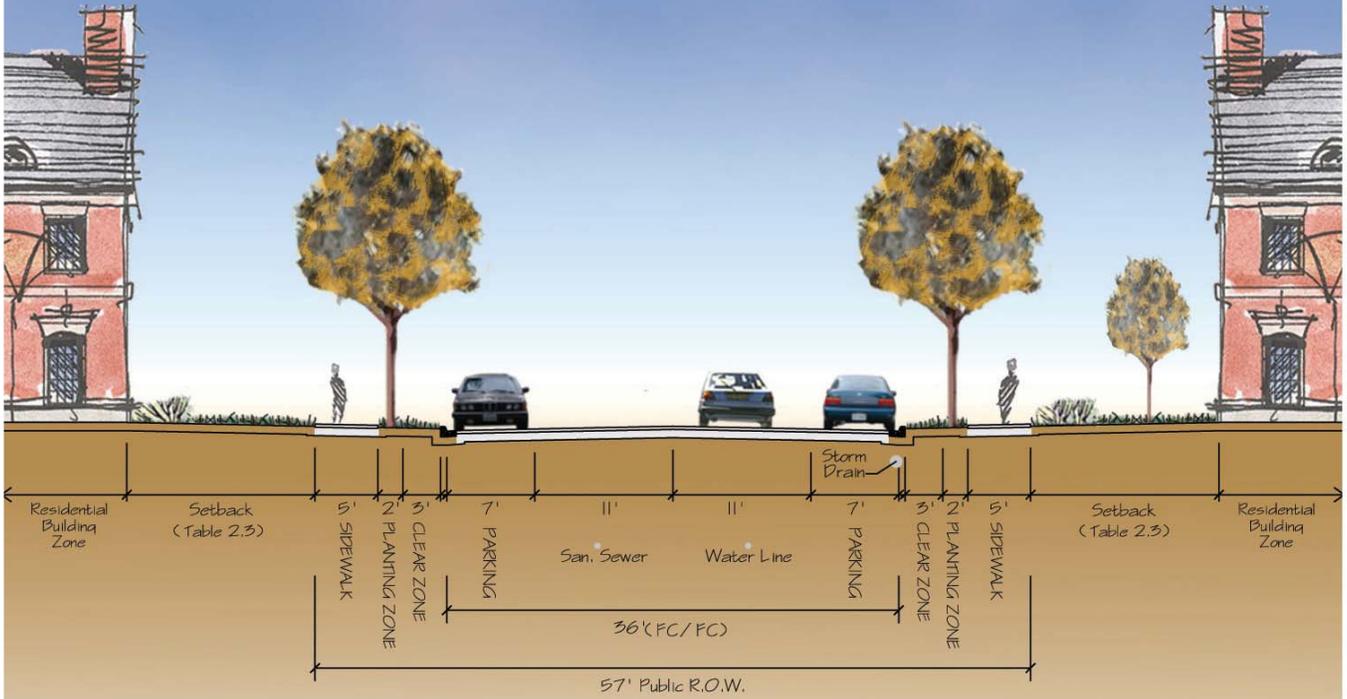
Figure 4.2 Street Map Diagram



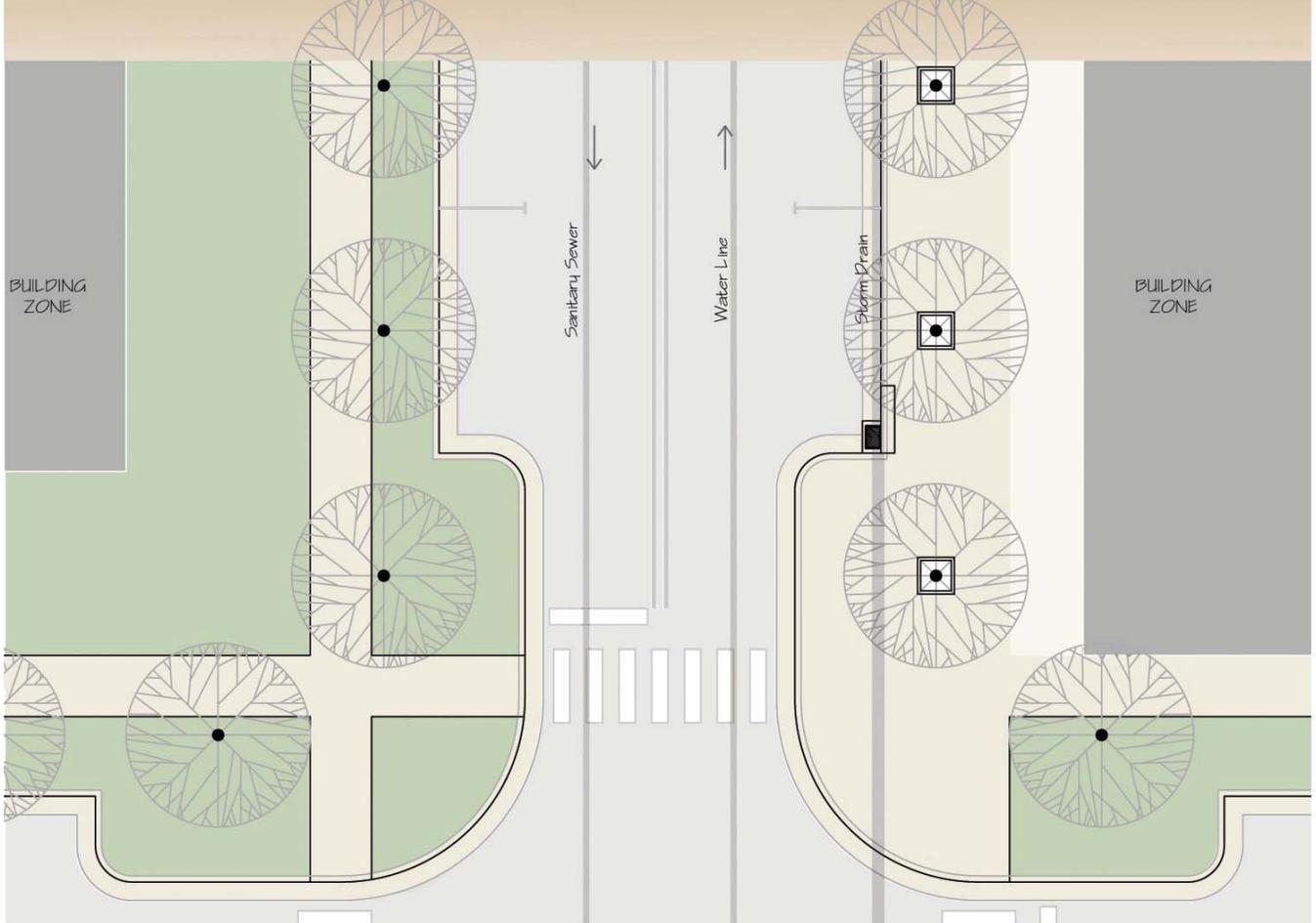
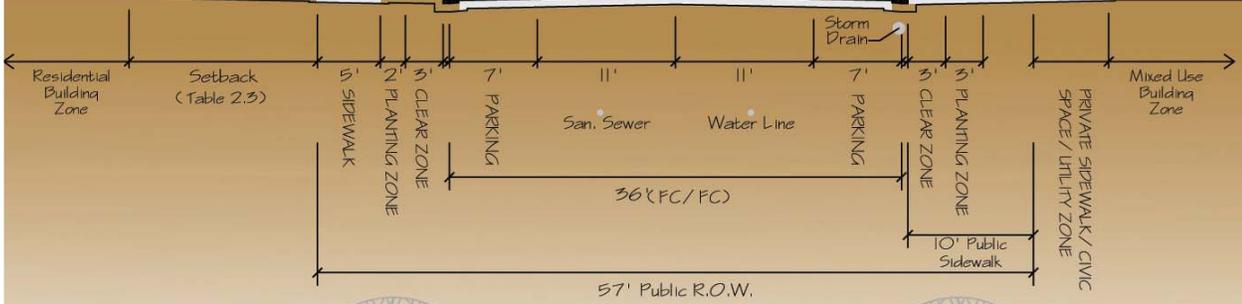
STREETS	Right-of-Way	Curb to Curb
 Major Village Street	57 feet	36 feet
 Minor Village Street	55 feet	34 feet
 Radney Road (<i>west of north street</i>)	60 feet	36 feet
 Private Residential Alley		

Streetscape Section I Major Village Street - Round Hill

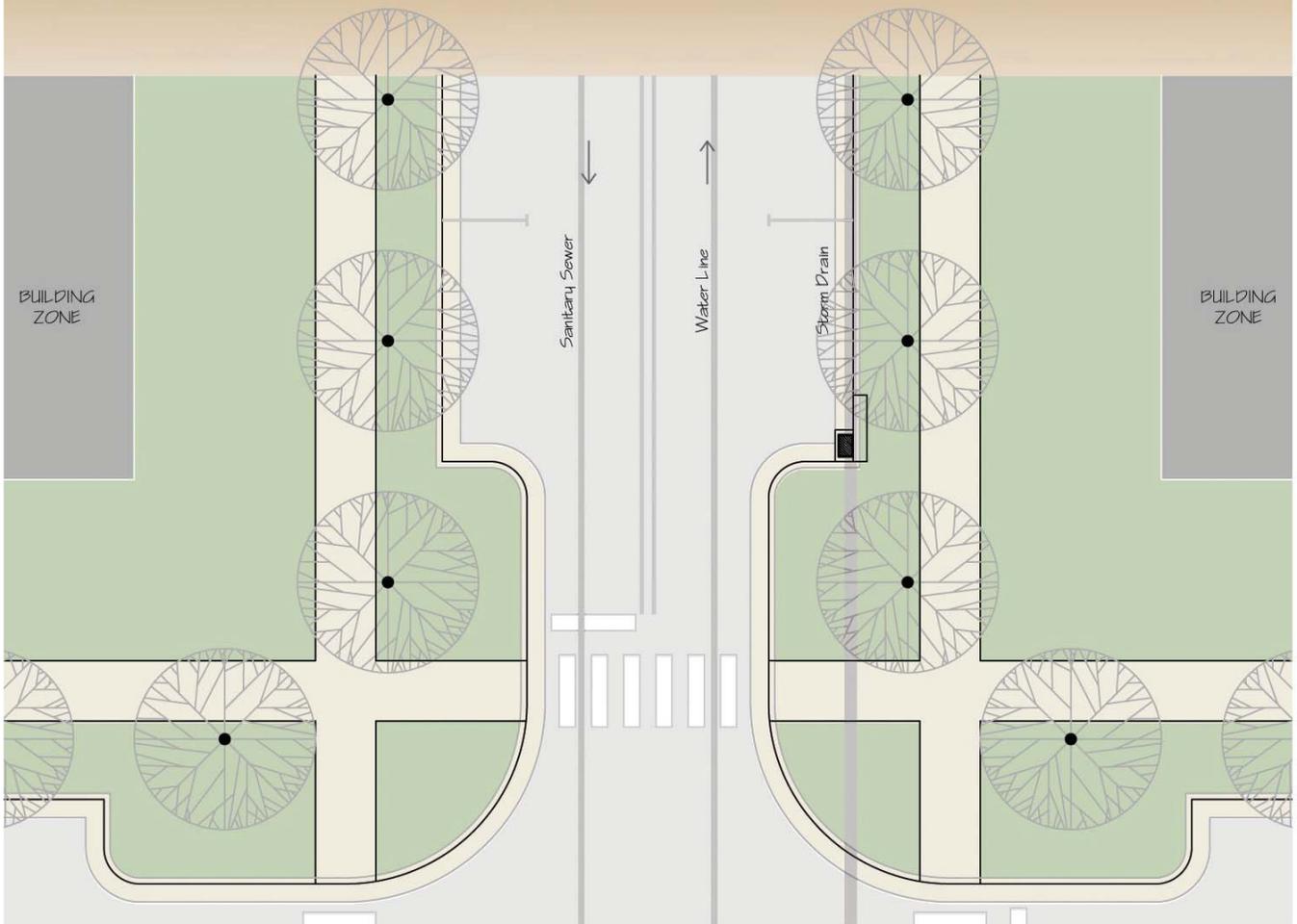
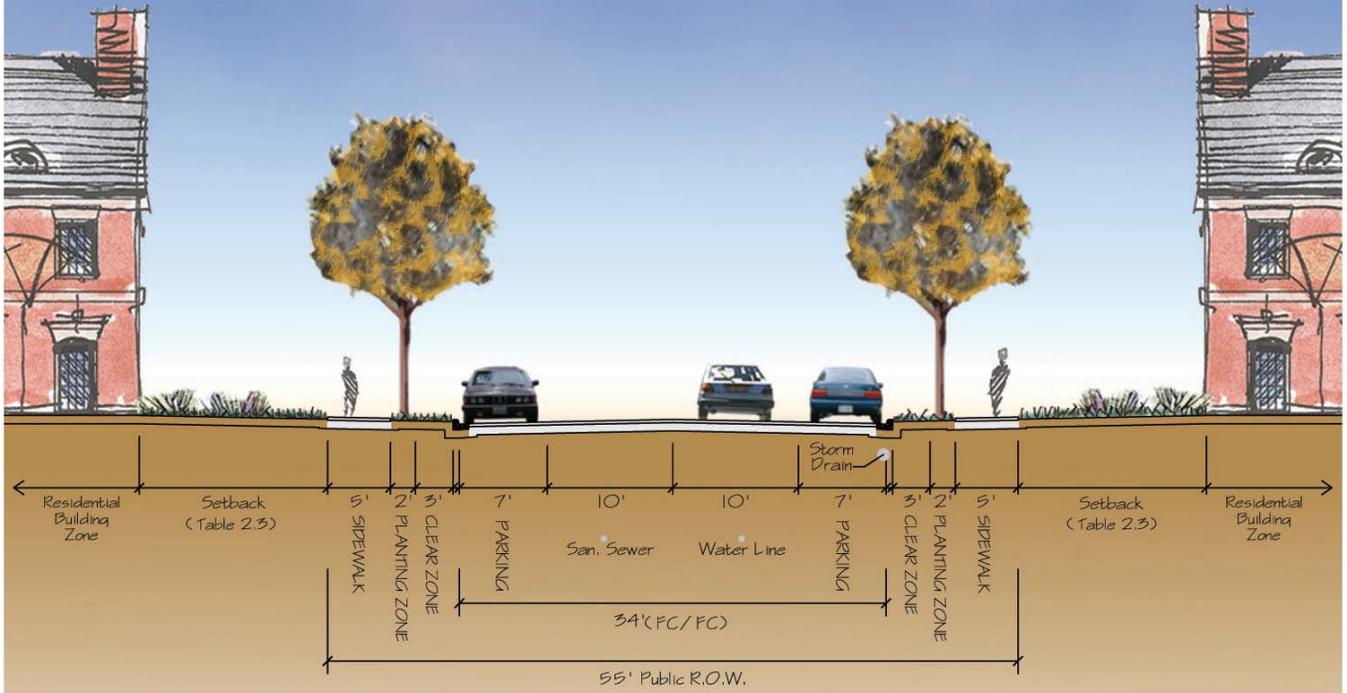
(for interior residential blocks greater than or equal to 500')



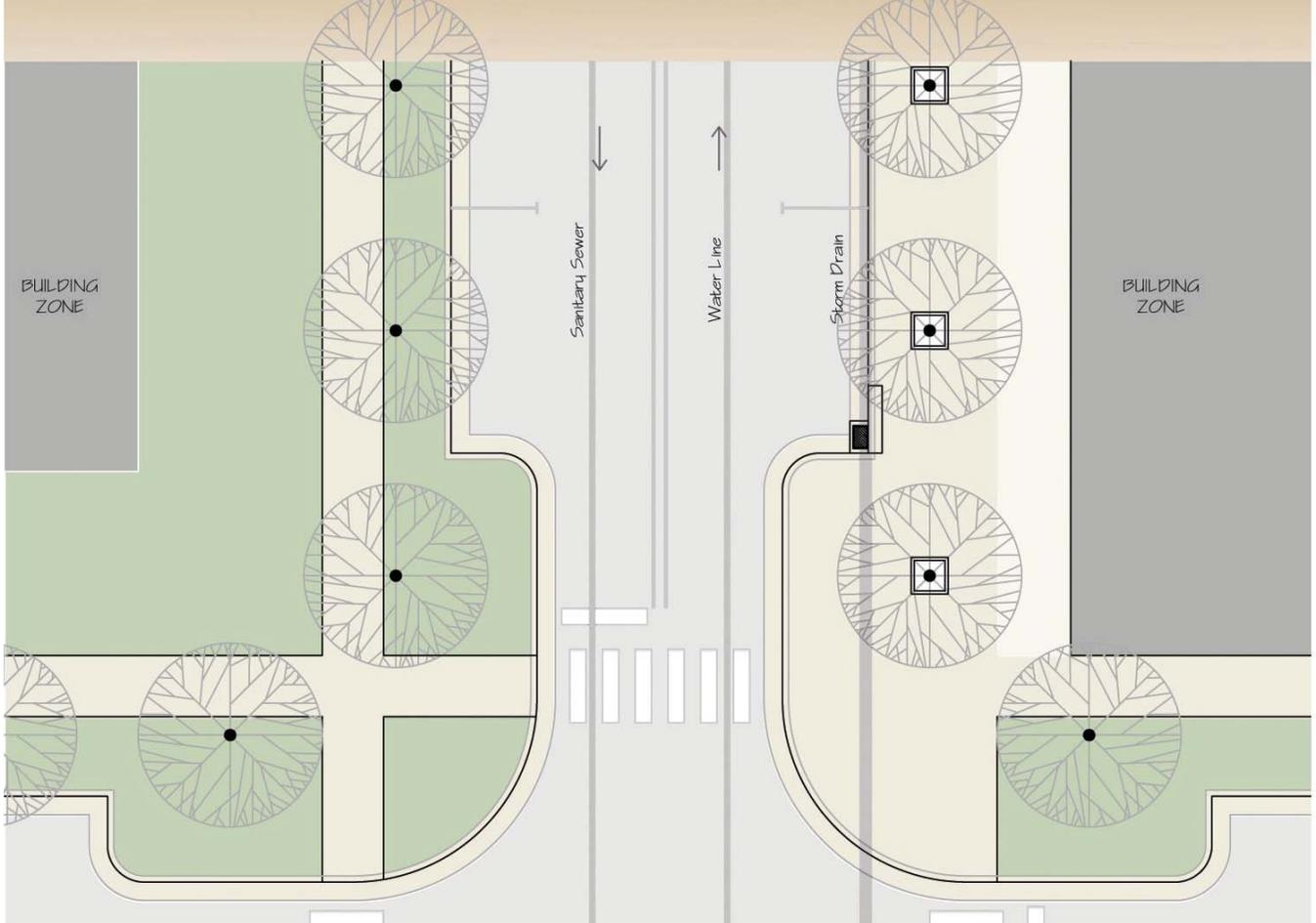
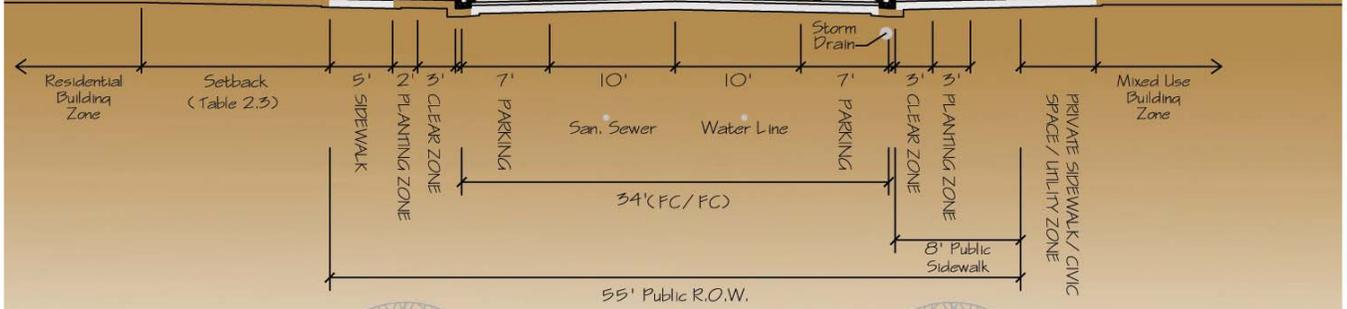
Streetscape Section 2 Major Village Street (Mixed Use Hybrid) - Round Hill



Streetscape Section 3
 Minor Village Street - Round Hill
 (for interior residential blocks less than 500' or with alley access)

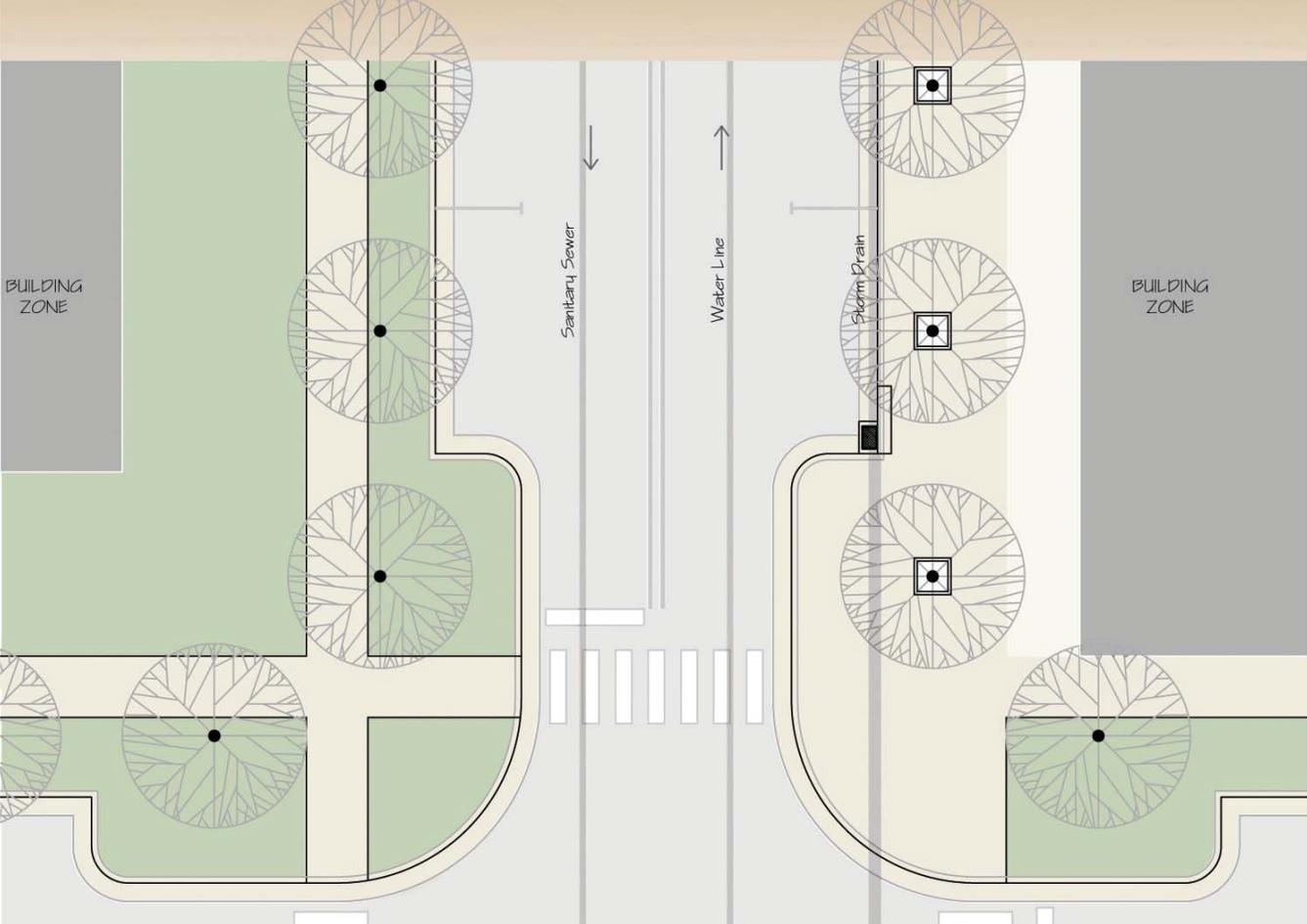
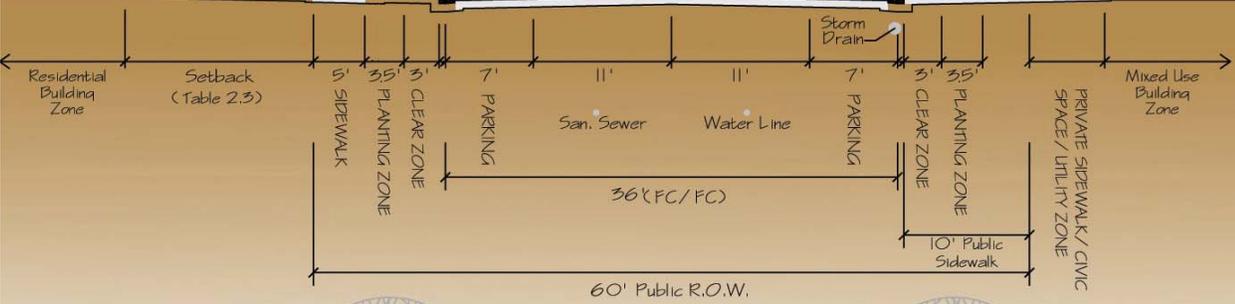
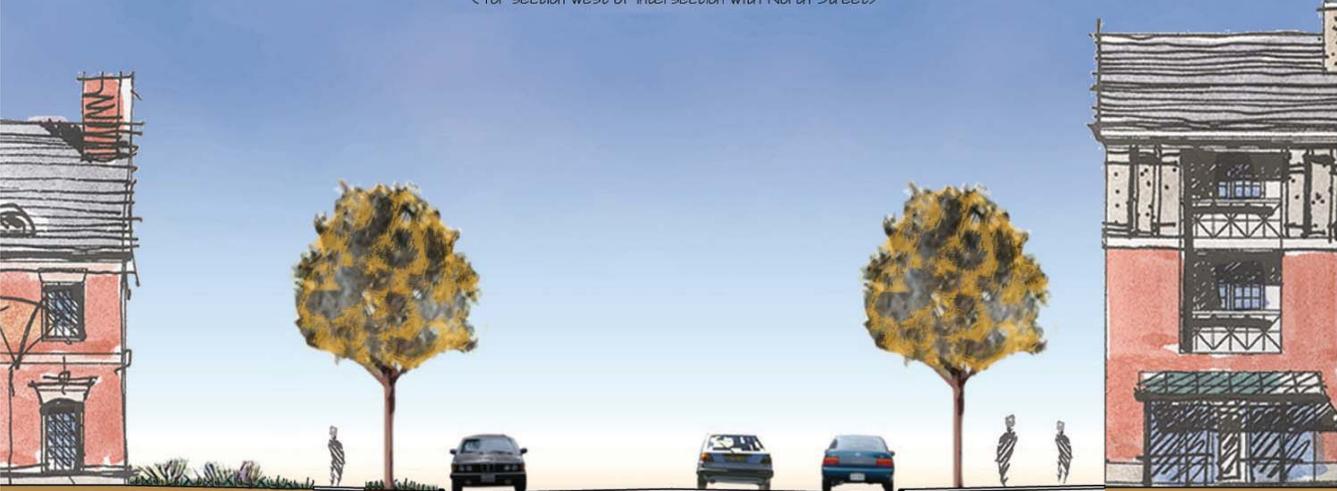


Streetscape Section 4
 Minor Village Street (Mixed Use Hybrid) - Round Hill

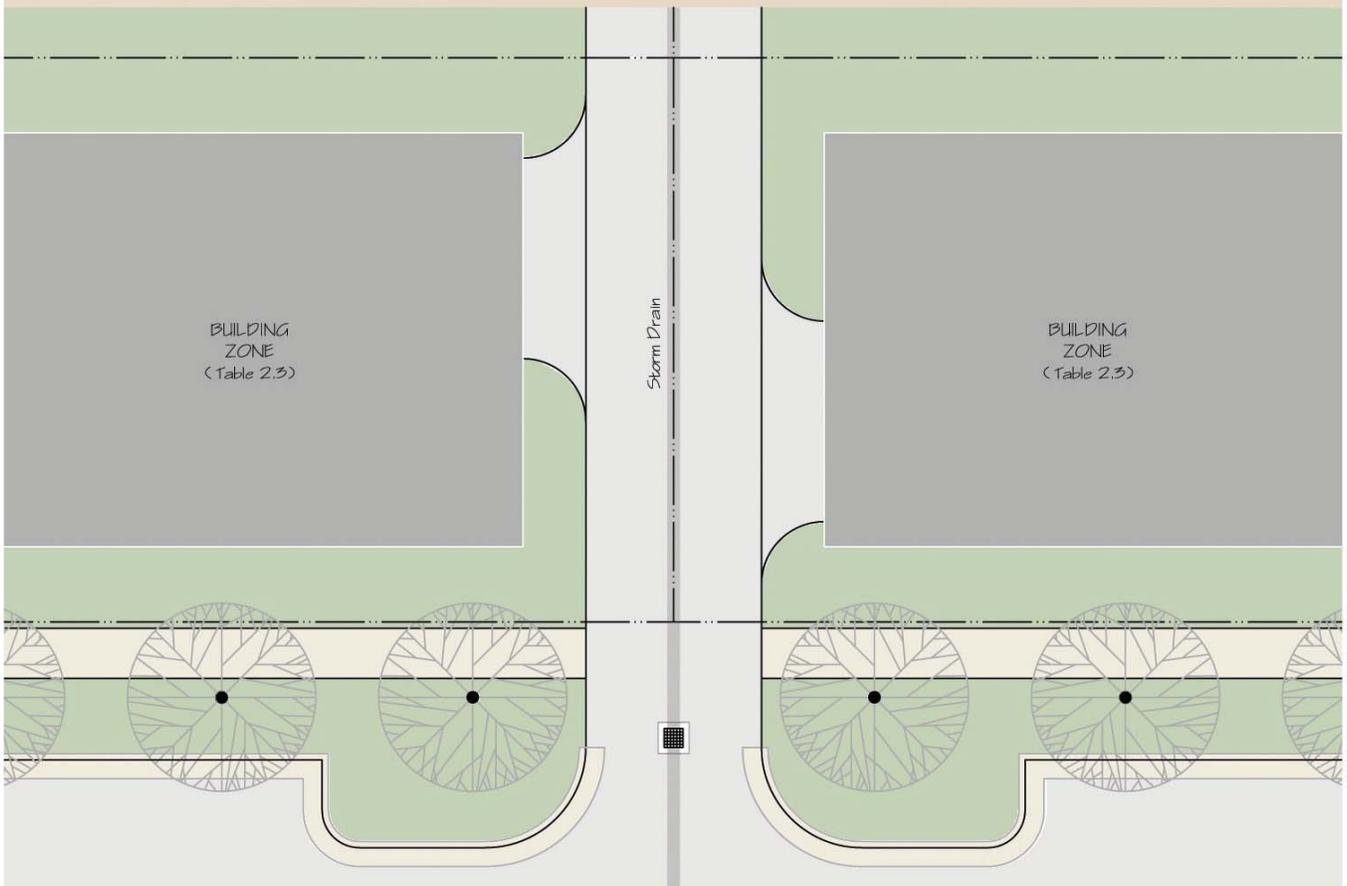
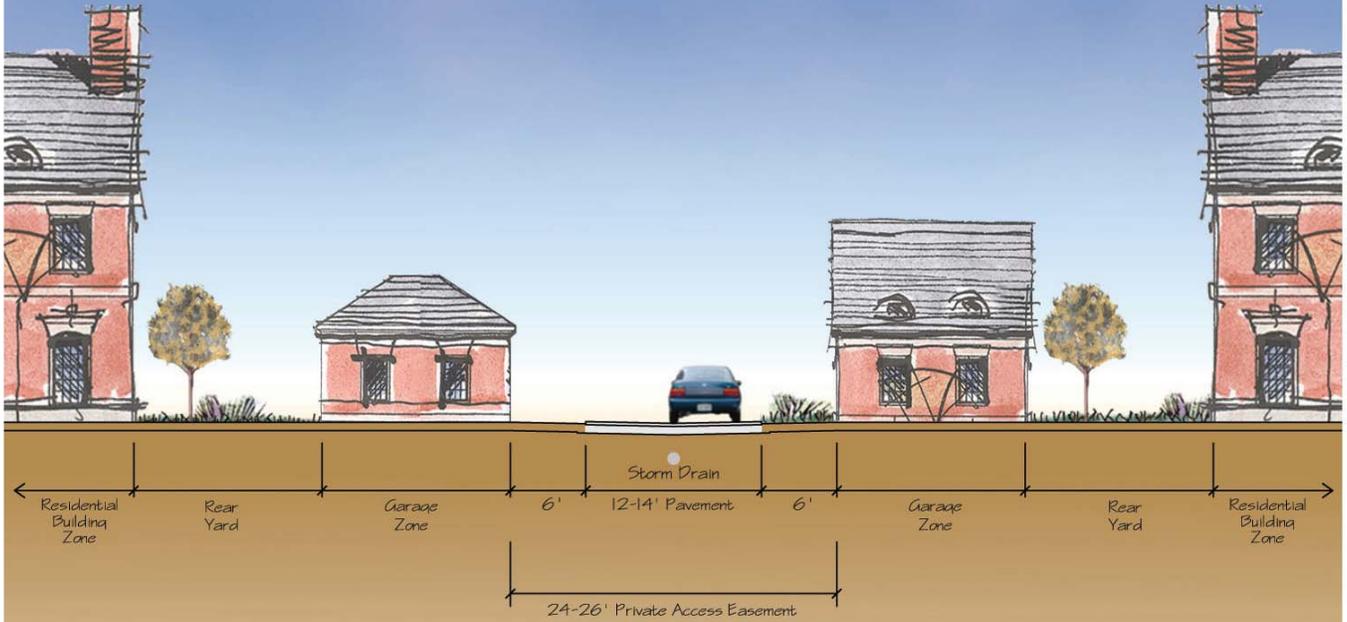


Streetscape Section 5 Radney Road - Round Hill

(for section west of intersection with North Street)



Streetscape Section 6
Private Residential Alley - Round Hill



Round Hill Code of Development

Figure 4.3 Parks, Open Space & Recreation Areas



T2 Open Space Areas
 - active and passive open space and recreation
 - 34.9 acres (25.9% of total)

Parks, Plazas & Landscape
 - additional neighborhood greenspace
 - subject to final site plan

Green Streetscapes
 - landscape and street trees
 - included in street section design

Round Hill Code of Development

Figure 4.4 Schematic Infrastructure Plan - Domestic Water



DOMESTIC WATER SERVICE

-  Domestic Water Main
-  Connection to Existing Service
-  Future Extension

Round Hill Code of Development

Figure 4.5 Schematic Infrastructure Plan - Sanitary Sewer



SANITARY SEWER SYSTEM

- Sanitary Sewer Main
- Manholes
- Force Main
- Direction of Flow

Round Hill Code of Development

Figure 4.6 Schematic Infrastructure Plan - Stormwater Management



STORMWATER MANAGEMENT AND BEST MANAGEMENT PRACTICES

-  Pre-Development Drainage Divides
-  Stormwater Management / BMP Facilities
-  Low Impact Development (LID) Techniques

Section 5:

Transect Zone Regulations: T2, T3, T4, T5A, T5B & SD 15

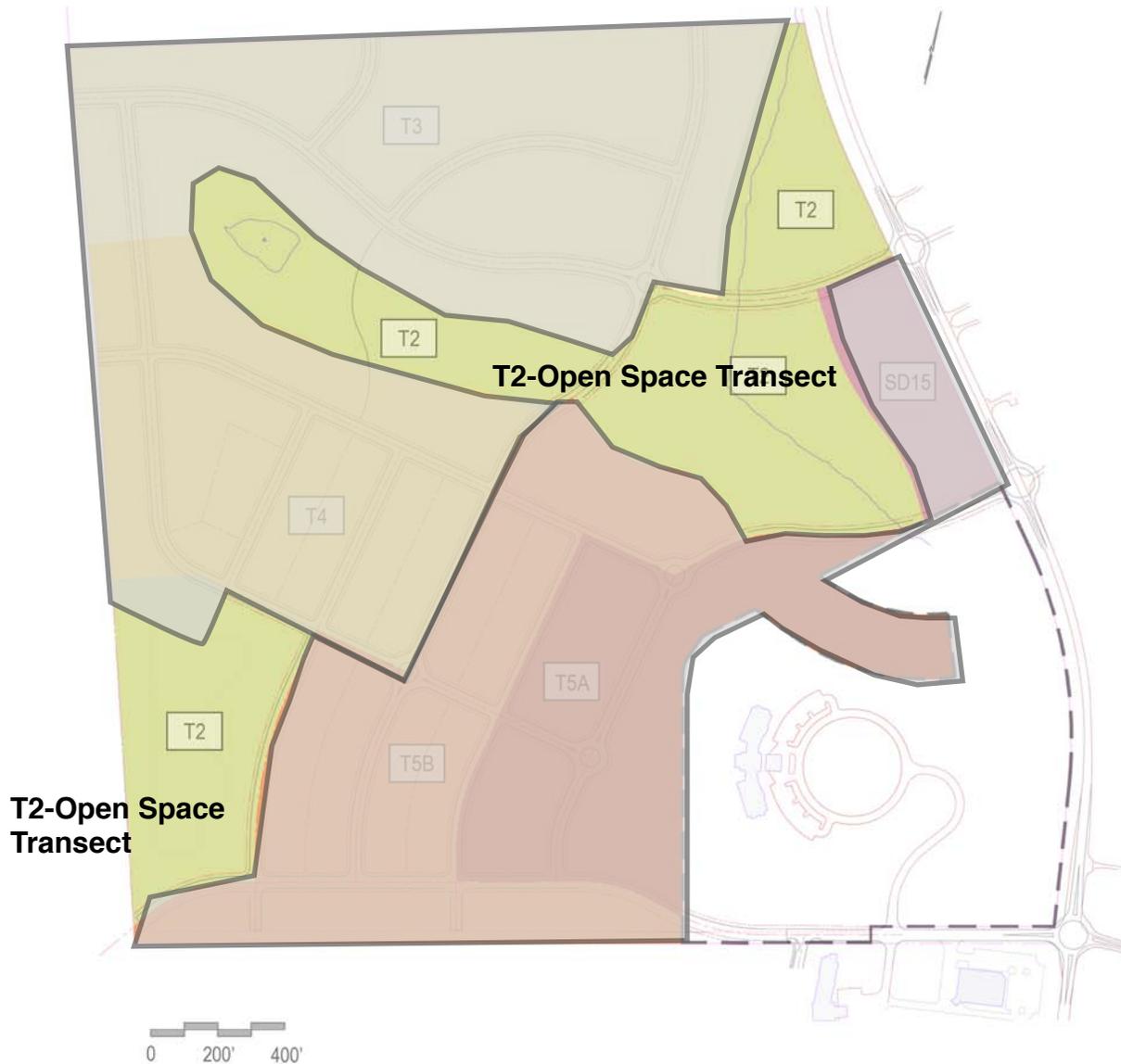


Round Hill/UNO-TND District

The Town of Orange

T2-Open Space Transect Zone

34.94-acres



**T2-Open Space
Transect**

Round Hill/UNO-TND District

The Town of Orange

T-2 Open Space Transect Zone

Intent

The T2 Open Space Zone transect represents the land area within Round Hill that is dedicated to open space, civic areas, and recreational use for the enjoyment of the community's residents and visitors. While serving as a conservation area to protect critical environmental resources, the T2 transect also allocates discrete locations for both passive and active recreation. In addition, locations within the T2 transect allow for improvements such as stormwater management, best management practices, buffer landscaping, and wetlands protection. The T2 Open Space Zone may also allow on a limited basis certain civic, public, institutional, and religious buildings. The sub-sections herein below provide the regulatory standards for potential buildings to be located within the transect. The specific improvements to be located within the individual T2 transects will be in accord with a master plan to be submitted by the project developer with the first site plan application for the project. It is recognized that the Round Hill developer may establish and impose more restrictive design guidelines for the T2 Open Space Zone as may be further imposed by the Round Hill Design Review Committee.

Area and Density

The approved land area and the general location of the T2 Open Space Zone is depicted on the Transect Map Diagram (Figure 4.1 in Section 4). No residential uses are permitted in the T2 Transect, and, therefore, density standards are not applicable to this district. The intensity of non-residential uses is not regulated, but the geometric configuration of buildings to be located within the T2 transect shall comply with the standards herein and further conditioned on Town requirements that may be imposed during the site plan review process. The photographs and illustrations that follow are advisory only and are intended to only illustrate the development standards for this zoning district. All building plans and designs as well as associated landscape and site plans and designs in T2 Open Space Zone are subject to site plan and subdivision review and approval.

Permitted Uses

The uses permitted by-right and permitted with a special use permit within the T2 Open Space Transect Zone shall be in accordance with the Table of By-Right and Special Permitted Uses (Section 3).

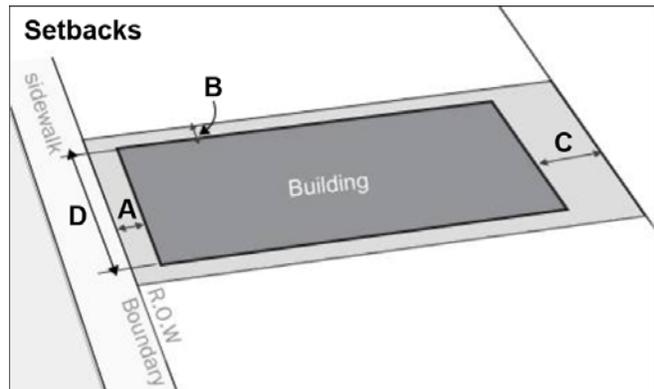
Development Standards

The regulatory development standards for permitted non-residential structures within the T2 Open Space Zone shall be regulated by the following tables.

Building Requirements for Non-Residential Uses

Setbacks

A	Street Front	20' min. (no maximum)
B	Side	20' min. (no maximum) 24' min. (corner lot)
C	Rear	25' min. 35' min. with alley
D	Facade buildout along frontage	not regulated not regulated



Lot Requirements

Lot Area

Permitted uses	per site plan and plat
Special permit uses	per site plan and plat

Lot Width

Permitted uses	per site plan and plat
Special permit uses	per site plan and plat

Lot Coverage

Permitted uses	per site plan and plat
Special permit uses	per site plan and plat

Density

established by site plan
not regulated

Block Length

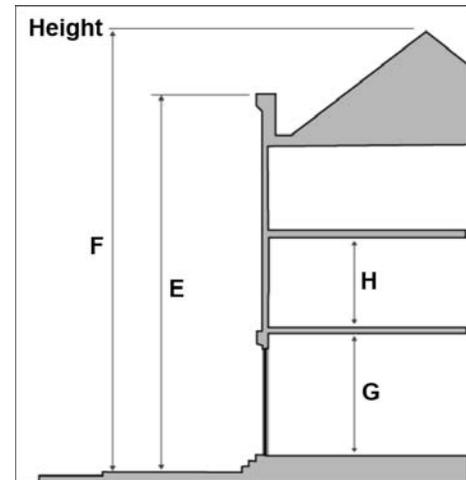
per Regulating Plan

Lot Access

each lot must front a
public street

Building Height for Non-Residential Uses

E	Building Height to eave	35' max.
F	Building Height overall	45' max.
	Increase in height	by special permit
G	First floor ceiling height	not regulated
H	Upper floor ceiling height	not regulated



Streets

The T2 Open Space Zone will be served by a variety of abutting public street types, each designed to accommodate expected traffic volumes and compliment planned adjacent land uses. Individual Streetscape Section designs specify the location and size of travel lanes, on-street parking areas, landscaping zones, and sidewalks, and locate street trees, crosswalks, and other streetscape features.

The following Streetscape Sections apply, in whole or in part, to activities in Round Hill's T2 zone:

- Major Village Street
- Major Village Street – Mixed Use
- Minor Village Street
- Minor Village Street – Mixed Use

The applicable streetscape sections for the T2 Open pace Zone are presented in the adopted Street Map Diagram (Regulating Plan) as depicted in Figure 4.2 in Section 4 of this ordinance.

Streetscape Standards

The streetscape standards herein below are provided to ensure the coherence of streets that abut the T2 Open Space Zone. No public streets are anticipated to be located within this transect. Internal parking may be located within the transect for the purpose of serving the residents and visitors to Round Hill. The majority of the parking needs for the transect will be met with the on-street parking located on abutting public streets.

1. Street Trees: A street tree plan shall be submitted with any public right of way infrastructure plan and plats. Street trees shall be installed by the developer along both sides of all streets within the T2 Open Space Zone. Street trees shall be spaced at regular intervals of not fewer than 30 and not greater than 50 feet as measured parallel to the centerline of the street. Street trees shall be limbed up so as to not interfere with pedestrian

or automobile travel. A minimum vertical clearance of 7 feet shall be provided above the sidewalk and 14 feet above the travel lanes of the street.

2. Sidewalks and Pedestrian Facilities: Sidewalks shall be installed by the developer along both sides of all streets within the T2-Open Space Transect, provided that a system of internal pedestrian trails may be substituted for a required sidewalk subject to review by the Community Development Director and approval by the Planning Commission.

Where the T2-Open Space Transect is contiguous with another transect, a public sidewalk shall be constructed within the public right of way on the opposite side of the street from the T2-Open Space Transect, provided that a system of internal pedestrian trails within the T2-Transect may be substituted for the required sidewalk on the T2 transect side of the street, subject to review by the Community Development Director and approval by the Planning Commission.

Unless otherwise provided, public sidewalks shall not be fewer than 5 feet in width and shall otherwise be installed in accord with the approved street section. Internal trails may be asphalt or other dust-less surface, subject to approval of the design section by the Town.

3. Outdoor Lighting: Site lighting associated with any approved building project within the transect shall be located and shall be of a design and height so as to illuminate only the interior of the property. A plan for the outdoor lighting design for a lot or project within the T2 Open Space Zone shall be included with the site plan or lot development plan.
6. Landscaping: Landscaping on projects within the transect shall be compatible with the street tree planting plan on public streets. A plan for the private lot or project landscape design for a lot or project within the T2 Open Space Zone shall be included with the site plan or lot development plan, and shall be compatible with all Town of Orange landscape ordinances.
7. Street Lighting:
 - (a) A street lighting plan shall be included with the submission of public improvements plans for all streets within the UNO/Round Hill TND.
 - (b) Street lighting shall be installed by the developer concurrently with the development of the public right of way.
 - (c) Street lights shall be installed along both sides of all streets.
 - (d) Street lights shall be installed at least 12 feet and not more than 20 feet in height above grade and at regular intervals of not greater than 200 feet on center as measured parallel to the centerline of the street.
 - (e) Street light luminaire fixtures and post standards shall be compatible with the Town of Orange requirements or as may otherwise be approved by the Planning Commission.

Additional Transect Development Standards

1. Street Terminus and Cul-de-sacs: Any temporary, dead-end street must terminate in a hammerhead or other alternative turnaround design located within a dedicated public right of way, provided, however, that a permanent cul-de-sac may be permitted.
2. The approved location and acreage within the T2 Transect is indicated on the Transect Map Diagram (Figure 4.1 in Section 4). Minor adjustments to the transect boundary and acreage may be approved by the Community Development Director subject to demonstration of need by the applicant based on final site plan, civil, engineering or geophysical determinations. Any variation to the size of the T2 Transect that increases or decreases the gross transect area by $\geq 10\%$ shall be subject to review and approval by the Planning Commission.

Parking

1. On-street parking is permitted in accord with individual street sections approved with final site plan and subdivision plats.
2. Additional off-street parking shall be provided on the individual lot subject to the building permit application. Off-street parking access from a public street is subject to approval of a parking access plan to be provided with the submission of a final site plan and subdivision plat for a specific block and public street within the T2 Open Space Zone.
3. Parking space design requirements permitted and special use permit uses within the T2 transect shall meet the requirements of the Town's Zoning Ordinance for the particular use unless otherwise reduced by the Planning Commission upon review and approval of a parking study to be submitted by the applicant with the final site plan or subdivision plat. On-street and shared-parking opportunities shall be taken into consideration with the review of parking studies.

Utility Requirements

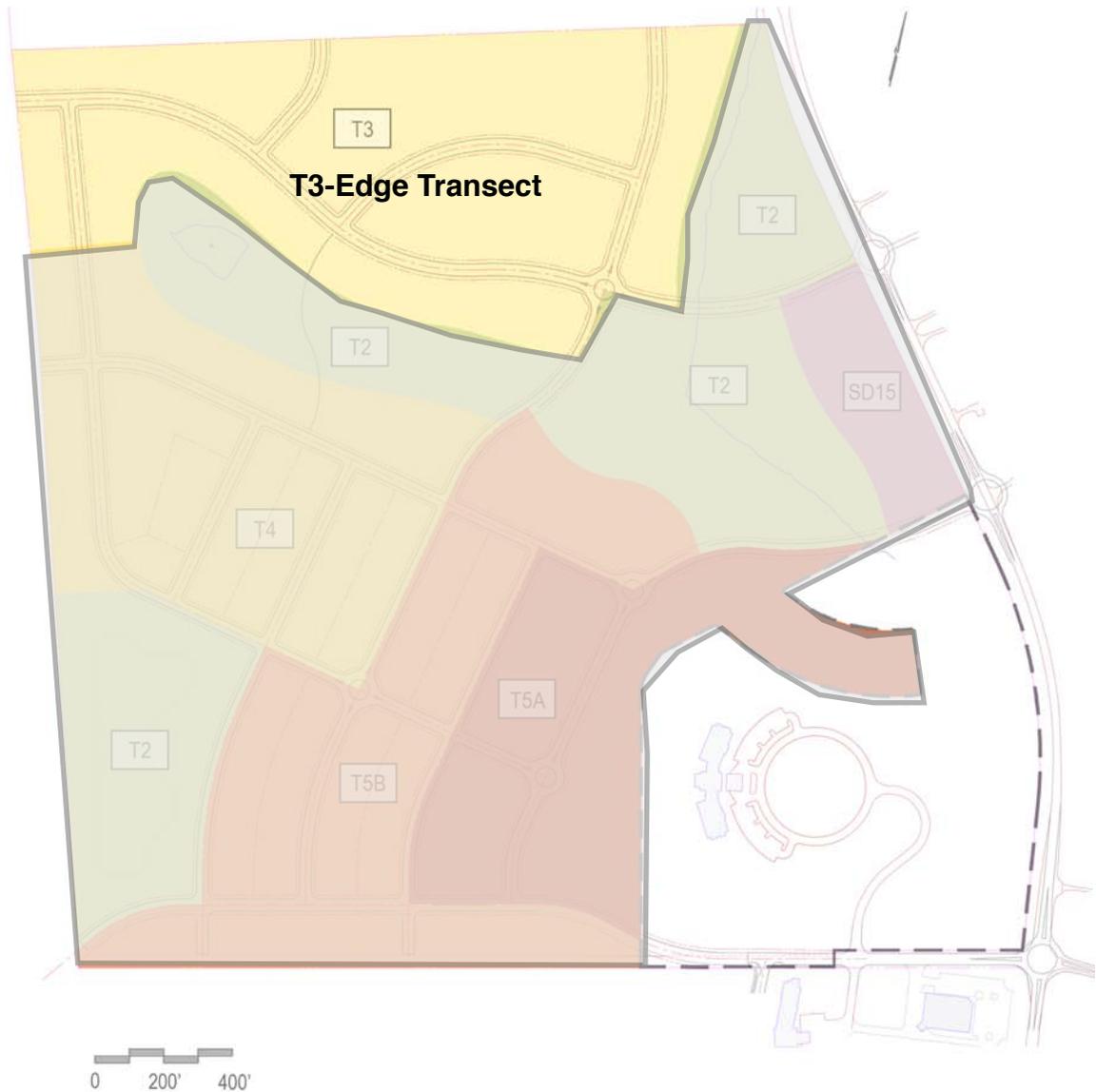
1. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public water system.
2. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public sewer system.
3. All utilities that serve a structure shall be underground. Temporary construction shall be exempted from this requirement.

Modifications and Variations from Development Standards

1. Modifications to the approved transect standards that are deemed “major modifications” by the Community Development Director, in consultation with the Town Attorney, shall require the applicant to request a formal amendment to the approved zoning of the TND District. An amendment to the zoning shall be processed by the Town in the same manner as an original zoning map amendment.
2. Modifications to the approved transect standards that are deemed “minor modifications” by the Community Development Director, in consultation with the Town Attorney, that are not otherwise delegated hereinabove to the Community Development Director may be approved by the Planning Commission upon submission of a statement of justification for the modification and the presentation of satisfactory alternatives by the applicant.
3. The Planning Commission shall consider the recommendation of the Community Development Director and other qualified sources in consideration of any minor modification from the approved transect standards hereinabove.
4. Requests for minor modifications from an approved transect standard shall be heard at a regularly scheduled meeting of the Planning Commission.

T3-Edge Transect Zone

31.93-acres



Round Hill/UNO-TND District

The Town of Orange

T3 Edge Transect Zone

Intent

The T3-Edge Transect Zone consists of medium density residential blocks relative to the balance of the Round Hill-TND District. The T3-Edge Transect Zone is established within the Round Hill-TND District to support the T5-Center zone with a more limited range of residential uses, including all single family detached dwelling types as well as townhouses and duplex units.

The T3 Transect Zone is to be developed in a TND pattern, where residential unit lot size and setbacks are greater than those in the T4 and T5 Transects. The sizing of blocks and alignment of interior streets shall be consistent with the regulations herein, and, further, shall respect and accommodate the natural terrain conditions of the transect. This zone is complemented by open space provided in the T2-Edge zones. The sub-sections herein below provide the regulatory standards for the transect. It is recognized that the developer may establish and impose more restrictive design guidelines. The photographs and illustrations that follow are advisory only and are intended to only illustrate the development standards for this zoning district.

Area and Density

The approved land area and the general location of the T3 Transect Zone is depicted on the Round Hill TND Transect Map Diagram (Exhibit 4.1 in Section 4). The minimum and maximum residential density, expressed in dwelling units per acre, is, as follows:

<i>Minimum Residential Density:</i>	<i>3.0 dwelling units per acre.</i>
<i>Maximum Residential Density:</i>	<i>6.0 dwelling units per acre.</i>

Where a non-residential or supporting use in the T3 Transect Zone is permitted by right or by special use permit (such as a church, civic, or recreational building), the density for such use is regulated by conditions established by the Town with subdivision plat and final site plan requirements. Mixed-use buildings and commercial building uses are not permitted in the T3 Transect.

(See “Additional Transect Development Standards” section for the method of calculation of the minimum and maximum permitted number of residential dwelling units and the maximum gross floor area of non-residential within the T3 Transect Zone.)

Permitted Uses

The uses permitted by-right and permitted with a special use permit within the T3-Edge Transect Zone shall be in accordance with the Table of By-Right and Special Permitted Uses (Section 3).

Mix of Uses

With respect to land use mix, only residential shall be developed in the T3 Transect Zone, provided that certain supporting uses, interior pocket parks, civic space, and recreation areas are counted in the total transect land area. See Table of By-Right and Special Permitted Uses (Section 3).

The following represents the permitted range in the allowable mix of uses, expressed as a percentage of the total T3 Transect Zone land area:

<i>Minimum Residential Percentage:</i>	<i>100%</i>
<i>Maximum Residential Percentage:</i>	<i>100%</i>
<i>Minimum Non-Residential/Mixed-Use Percentage:</i>	<i>0%</i>
<i>Maximum Non-Residential/Mixed-Use Percentage:</i>	<i>0%</i>

See Appendix A for an illustrative example for the calculation of minimum and maximum permitted mix of residential and non-residential/mixed uses in the T3 Transect Zone.

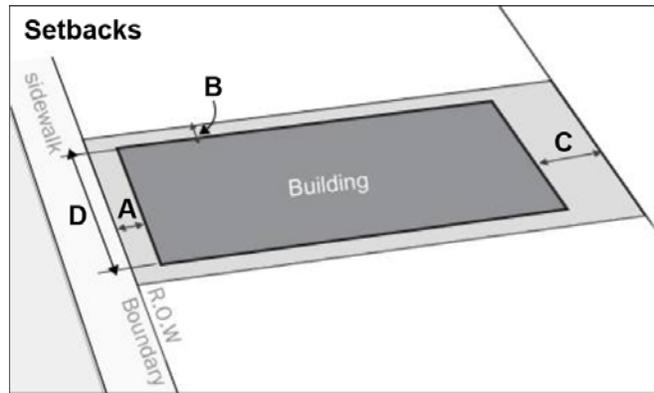
Development Standards

The regulatory development standards for the T-3 Edge Transect are regulated by the following tables.

Building Lot Requirements

Setbacks

A	Street Front	12' min. - 30' max.
B	Side	5' min. - 40' max. combined 8' min. - 24' max. (corner lot)
C	Rear	15' min. 20' min. with alley
D	Facade buildout along frontage	50% min. front street 30% min. side street



Lot Requirements

Lot Area (See Table 7.1 for lot type & allocation guidelines)

Single family	3600 sf. min.
Townhouse	1500 sf min.
Multi family	per site plan
Non-residential	per site plan

Lot Width

Single family	36' min.
Townhouse	16' min.
Multifamily	per site plan
Non-residential	per site plan

Lot Depth

Townhouse	75'
Single Family	85'
Multifamily	per site plan
Non-residential	per site plan

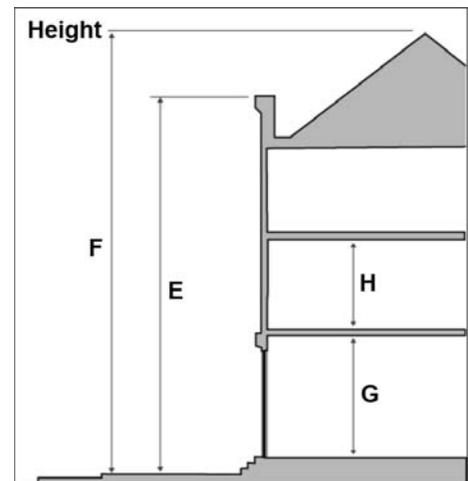
Lot Coverage	65% max. single family
	85% max. towns/multi-family

Block Length	600' max.
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Lot Access	Each lot must front a public street
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Building Height

E	Building Height to eave	30' max. (1-3 stories)
F	Building Height overall	40' max. (1-3 stories)
G	First floor ceiling height	9' min. clear
H	Upper floor ceiling height	8' min. clear



Residential Encroachment

Front Porch (unenclosed) max. 4' into minimum residential front setback

Chimney max. 1' into minimum residential side setback

(applicable to single family and townhouse units.)

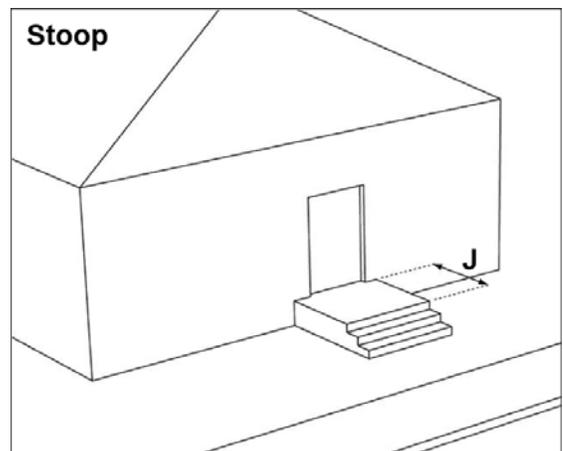
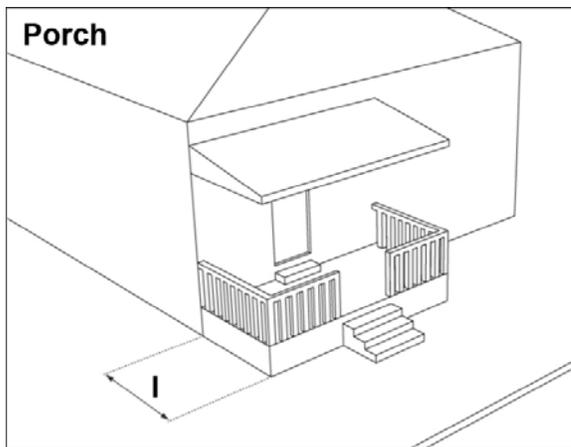
Frontage Types

Porch

I Depth 8' min.

Stoop

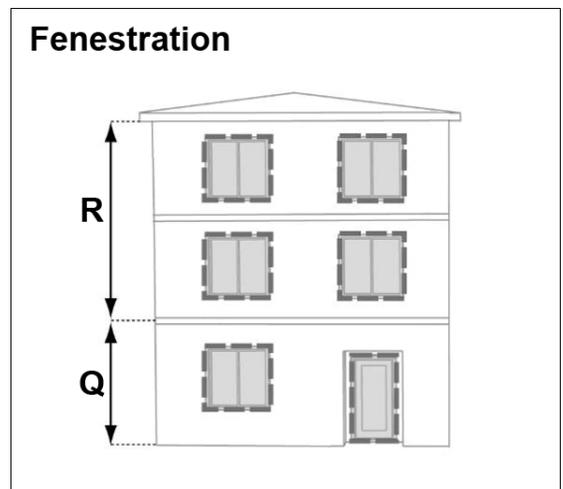
J Depth 5' min.



Fenestration

Q Street Level 30% min.

R Upper Floors 30% min. – 70% max.

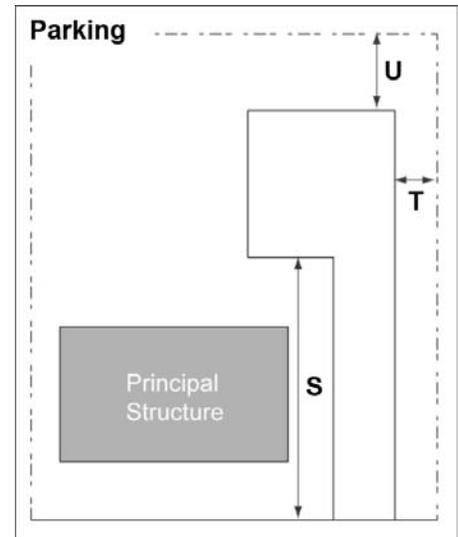


Parking

S	Front Setback	15' behind front facade
T	Side Setback	0' min. (5' for corner lots)
U	Rear Setback	3' min. or 15' from center line of alley.

Accessory Structures

Setbacks	
Front	45' min.
Side	0' min. 0' min. – 39' max. (corner lot)
Rear	5' min. 8' min adjacent to T4 18' min. from centerline for garage off alley
Height	No taller than primary build.
First floor ceiling	9' min. 10' min. (garage)



Streets

The Round Hill development will be served by a variety of street types, each designed to accommodate expected traffic volumes and compliment planned adjacent land uses. Individual Streetscape Section designs specify the location and size of travel lanes, on-street parking areas, landscaping zones, and sidewalks, and locate street trees, crosswalks, and other streetscape features.

The following Streetscape Sections apply, in whole or in part, to development in Round Hill's T3 zone:

- Major Village Street
- Minor Village Street
- Private Residential Alley

The applicable streetscape sections is presented in the adopted Street Map Diagram (refer to Exhibit 4.2 in Section 4 of this ordinance.)

Streetscape Standards

The streetscape standards herein below are provided to ensure the coherence of streets within the T3-Edge Transect zone. In addition to other sections of this ordinance, these standards serve to assist building owners and operators with the understanding of the relationship between the street and privately-owned lots. These standards are intended to establish an environment that encourages and facilitates pedestrian activity. The streetscape standards for the T3-Edge Transect zone are prescribed below, provided that the requirements of this subsection shall apply to a subdivision and/or development subject to the requirement for a site development plan as well as to lot development plans for individual detached and duplex dwellings. Variations from these standards may be approved by the Planning Commission upon submission of a statement of justification and the presentation of satisfactory alternatives by the applicant.

1. Street Trees:

(a) A street tree plan shall be submitted with any public right of way infrastructure plan and plats.

(b) Street trees shall be installed by the developer along both sides of all streets within the T3 Transect zone.

(c) Street trees shall be spaced at regular intervals of not fewer than 30 and not greater than 50 feet as measured parallel to the centerline of the street.

(d) Street trees shall be limbed up so as to not interfere with pedestrian or automobile travel. A minimum vertical clearance of 7 feet shall be provided above the sidewalk and 14 feet above the travel lanes of the street.

2. Sidewalks and Pedestrian Facilities:

(a) Sidewalks shall be installed by the developer along both sides of all public streets within the T3 Transect zone. Where the street section is contiguous with a T2-Open Space Transect, a system of internal pedestrian trails located interior to the T2-Transect may be substituted for the required sidewalk on the T2 transect side of the street, subject to review by the Community Development Director and approval by the Planning Commission.

(b) Sidewalks shall not be fewer than 5 feet in width when located within a public right of way and shall otherwise be installed in accord with the approved street section. Sidewalks shall otherwise be designed and constructed to meet all applicable handicap access regulations.

(c) Consistency of paving design shall be required within the development project.

3. Alleys: The private access easement for any alley shall be not fewer than 16 feet and not more than 24 feet in width. Alley easements shall be platted and recorded prior to the sale of any lot that would be served by the alley. The oversight and maintenance of alleys shall

be the responsibility of a property owners' association that shall be established prior to the sale of the first lot within the section of development in which the alley is to be located.

4. Common Civic and Open Space within Transect Zone:

(a) A minimum of one tot lot or playground shall be developed on a separate lot reserved and maintained for community use within each transect zone. (*See "Parks, Open Space, and Recreation Areas" Map Exhibit 4.3 in Section 4*)

(b) At least five percent (5%) of the total land area included as part of an individual development project involving the construction of twenty-five (25) or more townhouse or multifamily dwelling units shall be preserved as common open space within the transect. This provision shall not apply to individual single-family or duplex dwellings. (See Recreational Facilities Standards sub-section).

5. Private Property Lighting: Site lighting shall be located and shall be of a design and height so as to illuminate only the interior of the property. A plan for the outdoor lighting design for a lot or project within the T3-Edge Transect zone shall be included with the site plan or lot development plan.

6. Private Property Landscaping: Landscaping on individual lots and projects shall be compatible with the street tree planting plan on public streets. A plan for the private lot or project landscape design for a lot or project within the T3-Edge Transect zone shall be included with the site plan or lot development plan, and shall be compatible with all Town of Orange landscape ordinances.

7. Street Lighting:

(a) A street lighting plan shall included with the submission of public improvements plans for all streets within the Round Hill / UNO TND.

(b) Street lighting shall be installed by the developer concurrently with the development of the public right of way. Lighting shall be designed and installed to be fully shielded (i.e. full cutoff) and shall have a maximum lamp wattage consistent with the requirements of the Town of Orange Public Works Manual or as otherwise approved during the site plan review process.

(c) Street lights shall be installed along both sides of all streets.

(d) Street lights shall be installed at least 12 feet and not more than 20 feet in height above grade and at regular intervals of not greater than 200 feet on center measured parallel to the centerline of the street.

(e) Street light luminaire fixtures and post standards shall be compatible with the Town of Orange requirements or as may otherwise be approved by the Planning Commission.

Additional Transect Development Standards

1. Minimum 30 feet frontage for single family residential detached dwellings, where permitted, provided that cul-de-sac lots and “flag” (or “stem”) lots with less public street frontage may be approved on an individual basis by the Community Development Director. Public frontage requirements are not regulated for townhouse and duplex, but lot frontage geometry is subject to site plan review and approval. The total number of flag lots within the T3 Transect shall not exceed five percent (5%) of the total number of approved lots within the transect. A flag lot shall be limited to providing access to and accommodating a single dwelling unit. The entirety of the frontage and drive aisle portion of a flag lot shall be under individual ownership with its geometric configuration integral to that of the platted lot and building that it serves.
2. A residential lot development plan shall be required with zoning permit applications for single family detached and duplex dwelling units. The zoning permit shall be issued upon approval of the lot development plan by the Community Development Director. Townhouses, multifamily residences and other uses shall be subject to site plan and subdivision plat approval. See “SFD/Duplex Residential Lot Development Plan” subsection herein below.
3. Lot access: All single family detached residential lots which contain a residential dwelling or other permitted main building shall abut a public street.
4. Corner Lots: Of the two (2) sides of a corner lot, the front lot line shall be deemed to be the shorter of the two sides adjoining the street.
5. Street Terminus and Cul-de-sacs: Any temporary, dead-end street must terminate in a hammerhead or other alternative turnaround design located within a dedicated public right of way, provided, however, that a permanent cul-de-sac may be permitted.
6. Roof Pitch: Not regulated.
7. Accessory Structures: The footprint of an accessory structure shall not exceed 50 percent (50%) of the footprint of the principal dwelling up to a maximum area of 1,000 square feet. The height of the accessory structure shall not exceed the height of the principal dwelling. All accessory structures must be set back from the front lot line far enough so that at least 50 percent (50%) of the mass or depth of the principal structure is closer to the front lot line than is any portion of the accessory structure.

8. Calculation of Minimum and Maximum Residential Dwelling Count:

- MXPRD:** Maximum permitted residential dwellings (*in dwelling units*)
- MXPDP:** Maximum permitted density in transect (*in dwelling units per acre*)
- MNPRD:** Minimum permitted residential dwellings (*in dwelling units*)
- MNPDP:** Minimum permitted density in transect (*in dwelling units per acre*)
- TGLA:** Transect gross land area (*in acres*)
- TANR:** Transect area allocated to non-residential and mixed-use (*in acres*)

The maximum number of dwelling units permitted within the T3 Transect is calculated as follows:

$$\begin{aligned} & \textbf{Maximum Permitted Residential Dwellings} = \\ & \textbf{(Maximum Permitted Density in T3 Transect, in dwelling units per acre) X} \\ & \textbf{[(Total T3 Transect Gross Land Area, in acres) -} \\ & \textbf{(T3 Transect Area Allocated to Non-Residential and Mixed-Use, in acres)]} \end{aligned}$$

Abbreviated formula:

$$\text{MXPRD} = \text{MXPDP} \times (\text{TGLA} - \text{TANR})$$

The minimum number of dwelling units permitted within the T3 Transect is calculated as follows:

$$\begin{aligned} & \textbf{Minimum Permitted Residential Dwellings} = \\ & \textbf{(Minimum Permitted Density in T3 Transect, in dwelling units per acre) X} \\ & \textbf{[(Total T3 Transect Gross Land Area, in acres) -} \\ & \textbf{(T3 Transect Area Allocated to Non-Residential and Mixed-Use, in acres)]} \end{aligned}$$

Abbreviated formula:

$$\text{MNPRD} = \text{MNPDP} \times (\text{TGLA} - \text{TANR})$$

In transects where transference of dwelling units has been approved by the Town, the each lot development plan, site plan, and subdivision plat submitted to the Town shall include the following documentation:

- (a) a calculation of the cumulative dwelling units previously approved within the T3 Transect,
- (b) the cumulative number of dwelling units that have been applied to the individual project from the identified transferring transect, and
- (c) the cumulative number of of dwelling units that have been reduced in the transferring transect and the maximum number of permitted dwellings remaining in that transect.

Once the maximum allowable residential dwelling units within the transect have been approved for construction by the Town, no additional dwelling units may be approved to be constructed within the transect, provided that any subsequent modification to the maximum number of dwelling units shall be subject to a formal amendment to the Round Hill / UNO-TND Code of Development and the transect zoning regulations herein.

See Appendix A for an illustrative example for the procedure to calculate minimum and maximum permitted yield and mix of residential areas in the T3 Transect Zone.

9. Adjustment to Transect Area and Location: The approved location and acreage within the T3 Transect is as indicated on the Transect Map Diagram (Exhibit 4.1 in Section 4). Minor adjustments to the transect boundary and acreage may be approved by the Community Development Director subject to demonstration of need by the applicant based on final site plan, civil, engineering or geophysical determinations. Any variation to the size of the T3 Transect that increases or decreases the gross transect area by $\geq 10\%$ shall be subject to review and approval by the Planning Commission. Any change in transect area and acreage shall be documented by the applicant on a revised Transect Map Diagram. If approved, the revised Transect Map Diagram shall govern.

Recreational Facility Standards

Individual multi-family and townhouse projects of thirty (30) dwelling units or more in the T3 Transect Zone shall provide improved recreational space for the use of residents, including facilities such as playgrounds, tot lots, civic space, tennis courts, swimming pools and clubhouses. A property owners' (or homeowners') association agreement shall be responsible for the ownership and maintenance of common property within the project.

Applicants for townhouse and multifamily projects with fewer than thirty dwelling units shall demonstrate that the recreation and open space improvements in the planned and existing T2 Open Space transects are properly located and adequate to provide for the needs of project residents. If not adequately demonstrated, the Planning Commission, upon recommendation from the Community Development Director, may require recreational improvements within the proposed project.

All recreational and open space improvements shall meet the following minimum standards and supplementary requirements:

1. The size, type, and number of facilities provided shall be commensurate with the anticipated needs of the project's residents. Recreational improvements shall be coordinated with other open space and recreational improvements in the T2 Open Space Transect. Improvements to be located within the T2 Open Space Transect shall be taken into consideration in determining the relationship between supply and demand for said facilities.
2. In determining the extent of necessary facilities, the applicant shall meet with the Community Development Director prior to submission of final site plan in order to:
 - (a) coordinate improvements with the overall plan of open space and recreation improvements planned or existing in the T2 Open Space Transect Zone,
 - (b) coordinate improvements with the tot lot or recreation area planned or located within the transect in which the project is located, and
 - (c) finalize the specific recreational improvements to be located within the proposed project. The applicant shall provide construction drawings and specifications for proposed facilities at the time of site plan and subdivision plat submittal.
3. Slopes should not exceed ten percent (10%) and the area for the placement of recreational facilities.
4. The size and shape of each recreation area shall be adequate for the intended use.
5. Groundcover shall be appropriate for the use and shall consist of turf grass or contained ground cover such as pine bark mulch, shredded tires, or pea gravel.
6. Location, grading, and facility placement shall be compatible with adjoining uses, convenient to users, and suitable for parental supervision.
7. All recreational equipment and facilities shall be designed for regular and extended use. Recreational equipment and facilities shall be maintained in a safe and attractive condition and replaced as necessary. Maintenance shall be the responsibility of the property owners' or homeowners' association.
8. All required recreational facilities shall be completed or bonded for completion by the time of issuance of a Certificate of Occupancy for the units requiring the facilities.
9. A property owners' (or homeowners') association agreement and articles of incorporation shall be established and recorded with each project. Such agreements shall address the ownership and maintenance responsibilities of recreational land and improvements.

Parking

1. The TND District encourages creative parking strategies that are aimed at balancing gross parking requirements for the project with opportunities for shared parking, on-street parking, phasing of parking, and coordination with environmentally responsive, low impact development techniques. This objective may be pursued with at the time application for individual site plan and subdivision plat approval (*see 9. and 10. herein below.*)
2. On-street parking is permitted in accord with individual street sections included in this Code of Development and as may be subsequently approved with final site plan and subdivision plats for individual projects.
3. In addition to on-street parking, off-street parking shall be provided on the individual lot subject to the zoning permit application. Parking on private property may be accessed via a driveway or drive aisle directly from the public street on which the lot fronts or via an approved alley located within the private access easement. Off-street parking access from a public street is subject to approval of a final site plan and subdivision plat.
4. Garages serving single family detached residential dwellings accessed from a public street frontage must be set back a minimum of 15' from the front building line as established for the residential unit by the SFD/Duplex Lot Development Plan.
5. Residences located in the upper levels of mixed-use buildings shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces. Not more than seventy-five percent (75%) of the required residential off-street spaces may be located within a garage.
6. Townhouse and single family detached residences shall provide two (2) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage.
7. Freestanding multi-family residences shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage.
8. Applicants are encouraged to evaluate the benefits of shared parking and other parking reduction opportunities, where appropriate. The procedure for this is through the submission of a parking impact study. If the applicant elects to establish parking requirements based on a parking impact study, the study shall be subject to review and approval by the Planning Commission. The applicant shall prepare the parking impact study employing contemporary research and parking projection techniques, including studies prepared by the Urban Land Institute.

The parking impact study for a given project shall address: (a) planned and existing on-street parking supply, (b) shared-parking opportunities with other planned and existing residential and non-residential development projects, (c) mixed-use parking relationships,

(d) location of project within the transect, (e) development phasing, (f) coordination with runoff reduction techniques, and (g) demonstration of total parking adequacy to meet the marketplace demands of the transect and project. The project-specific scope of the parking impact study shall be established at a meeting with the Community Development Director. The parking impact study shall be prepared by a qualified professional.

9. The parking impact study shall be submitted to the Community Development Director for review and approval at a minimum of thirty (30) days prior to submittal of the final site plan or subdivision plat for the project. Upon review, the Community Development Director shall provide written comment to the applicant which shall be reflected in the final site plan.

Upon review of the parking impact study and the subsequent review of the final site plan or subdivision plat, the Planning Commission may act to approve, deny, or amend with appropriate conditions the applicant's request for a reduction in the number of parking spaces or other aspects of parking requirements that would otherwise be required by the application of Town parking standards.

SFD/Duplex Residential Lot Development Plan

1. A lot development plan is a requirement of a zoning permit application for a duplex or single family detached residential unit(s). The lot development plan, prepared in accord with the requirements of the Section 6 of this ordinance, shall be submitted to the Town with the zoning permit application. Multiple units may be included on a lot development plan.
2. The lot development plan shall be (a) submitted with the zoning permit application, (b) demonstrate compatibility with the requirements herein above, (c) provide a cumulative total of the number of dwelling units previously approved for the transect, (d) exhibit consistency with the approved final site plan, transportation and utility plans, and subdivision plat for the block or transect infrastructure in which the building is proposed, and (e) incorporate the documentation and calculations required in Additional Transect Development Standards, Sub-section 9.

Utility Requirements

1. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public water system.
2. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public sewer system.
3. All utilities that serve a structure shall be underground. Temporary construction shall be

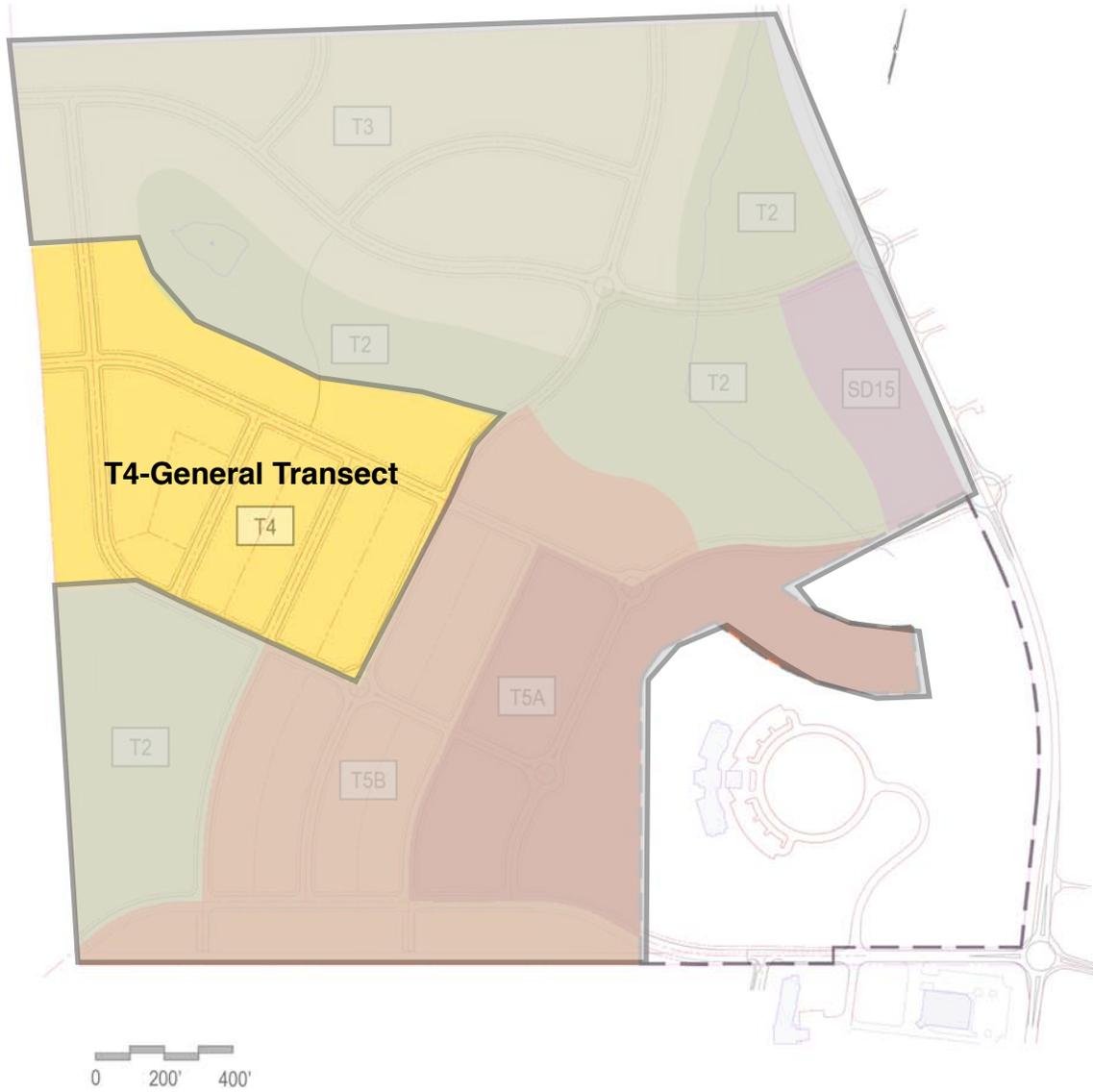
exempted from this requirement.

Special Administrative Standards: Modifications

1. The Town recognizes a process by which to obtain “major modifications” and “minor modifications” to the TND District transect zone standards contained herein. Modifications to the transect zone standards that are deemed “major modifications” by the Community Development Director, in consultation with the Town Attorney, shall require the applicant to request a formal amendment to the approved zoning of the TND District. An amendment to the zoning shall be processed by the Town in the same manner as an original zoning map amendment.
2. Modifications to the approved transect standards that are deemed “minor modifications” by the Community Development Director, in consultation with the Town Attorney, that are not otherwise delegated hereinabove to the Community Development Director may be approved by the Planning Commission upon submission of a statement of justification for the modification and the presentation of satisfactory alternatives by the applicant.
3. The Planning Commission shall consider the recommendation of the Community Development Director and other qualified sources in consideration of any minor modification from the approved transect standards hereinabove.
4. Requests for minor modifications from an approved transect standard shall be heard at a regularly scheduled meeting of the Planning Commission.

T4-General Transect Zone

22.87-acres



Round Hill/UNO-TND District

The Town of Orange

T4 General Transect Zone

Intent

The purpose of the T4-General Transect zone is to support development in the commercial core with a full range of residential types and relatively limited mixed use (so as to not detract from the centers). The physical form and land uses situated along the non-commercial urban and sub-urban thoroughfares in this transect reflect the urban character of those medium-high to high density, residential areas. Also permitted is limited mixed use. This zone is complemented by open space provided in the T2 Open Space zones. The sub-sections herein below provide the regulatory standards for the transect zone. It is recognized that the developer may establish and impose more restrictive design guidelines. The photographs and illustrations that follow are only intended to illustrate the development standards for this district.

Area and Density

The approved land area and the general location of the T4-Center Transect Zone is depicted on the Transect Map Diagram (Exhibit 4.1 in Section 4). The minimum and maximum residential density, expressed in dwelling units per acre, is, as follows:

Minimum Residential Density: ***3.0 dwelling units per acre.***
Maximum Residential Density: ***6.0 dwelling units per acre.***

The minimum and maximum non-residential density, expressed in total non-residential and mixed-use floor area ratio (FAR), is, as follows:

Minimum Non-Residential/Mixed-Use Density: ***0 FAR***
Maximum Non-Residential/Mixed-Use Density: ***0.5 FAR***

(See “Additional Transect Development Standards” section for the method of calculation of the minimum and maximum permitted number of residential dwelling units and the minimum and maximum gross floor area of non-residential and mixed-used development within the T4 Transect Zone.)

Permitted Uses

The uses permitted by-right and permitted with a special use permit within the T4-General Transect Zone shall be in accordance with the Table of By-Right and Special Permitted Uses (Section 3).

Mix of Uses

A mix of residential and non-residential/mixed-uses shall be developed in the T4 Transect Zone. The following represents the permitted range in the allowable mix of uses, expressed as a percentage of the total T4 Transect Zone land area.:

<i>Minimum Residential Percentage:</i>	90%
<i>Maximum Residential Percentage:</i>	100%
<i>Minimum Non-Residential/Mixed-Use Percentage:</i>	0%
<i>Maximum Non-Residential/Mixed-Use Percentage:</i>	10%

See Appendix A for an illustrative example for the calculation of minimum and maximum permitted mix of residential and non-residential/mixed uses in the T4 Transect Zone.

Development Standards

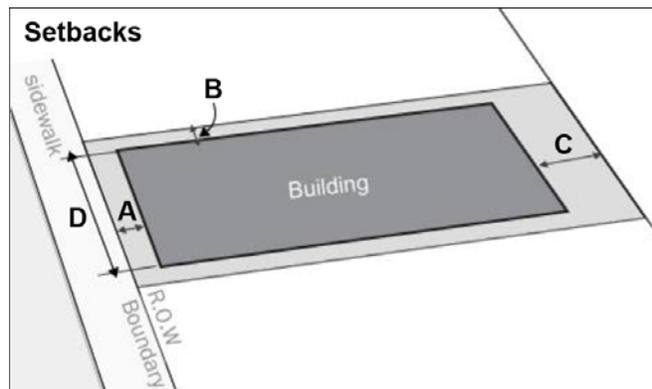
The general development standards in the T4 District are defined by the following tables and illustrations.

Building Lot Requirements

Setbacks

A	Street Front	12' min. - 20' max.
B	Side	5' min. - 35' combined max. 6' min. - 18' max. (corner lot) 0' min. (commercial or town-house; 5' min at corner lot)
C	Rear	15' min. 20' min. with alley
D	Facade buildout along frontage	50% min. front street 30% min. side street

Maximum building footprint per non-residential establishment: 4000 square feet gross floor area on first floor, provided that establishments that exceed 4000 square feet gross floor area shall require individual Special Use Permit approval.

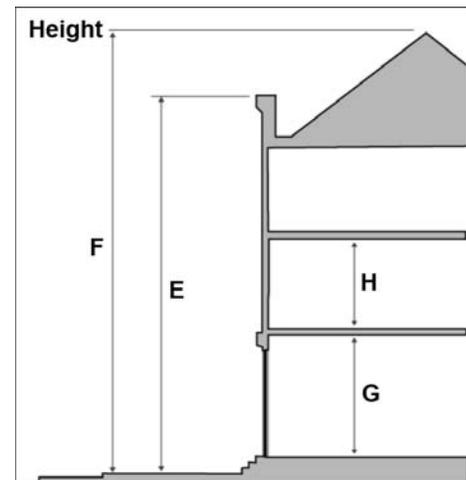


Lot Requirements

<u>Lot Area (See Table 7.1 for lot type & allocation guidelines)</u>	
Single family	3600 sf. min.
Townhouse	1500 sf. min.
Multi family	per site plan
Non-residential	per site plan
<u>Lot Width</u>	
Single family	36' min.
Town house	16' min.
Multifamily	per site plan
Non-residential	per site plan
<u>Lot Depth</u>	
Townhouse	75'
Single Family	85'
Multifamily	per site plan
Non-residential	per site plan
Lot Coverage	70% max. single family 90% max. other
Commercial footprint	4000 sf max/occupant
Block Length	600' max.
Lot Access	Each lot must front a public street

Building Height

E	Building Height to eave	40' max. (2-4 stories)
F	Building Height overall	50' max. (2-4 stories)
G	First floor ceiling height	9' min. civic / live-work 8' min. residential
H	Upper floor ceiling height	8' min. clear



Residential Encroachment

Front Porch (unenclosed)	4' into minimum residential front setback
Chimney	1' into minimum residential side setback (applicable to single family, townhouse, and live-work units.)

Frontage Types

Porch

I Depth 6' min. – 14' max.

Stoop

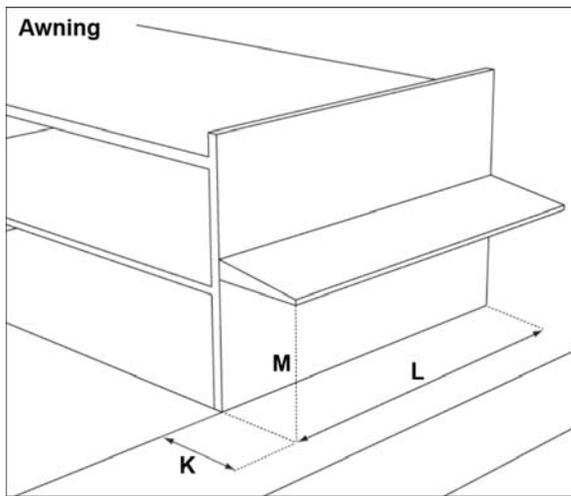
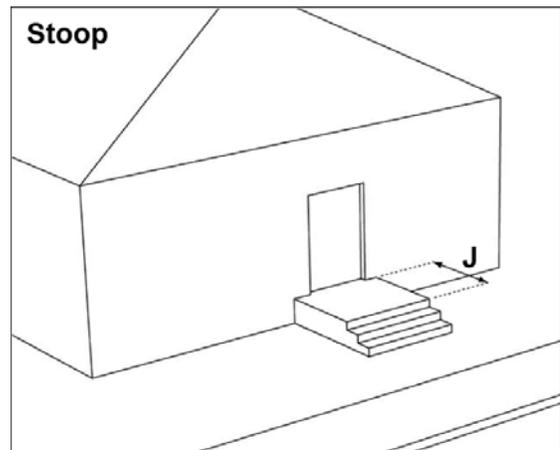
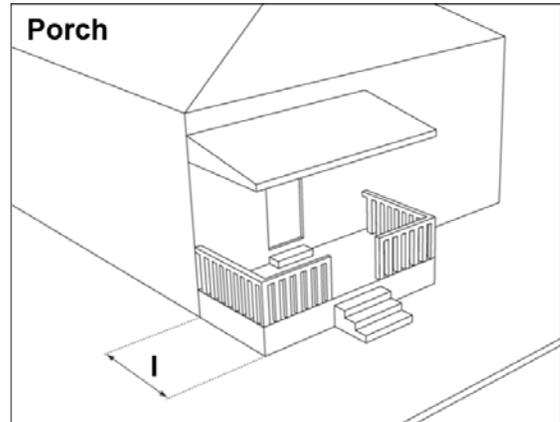
J Depth 5' min. – 14' max.

Awning

K Depth 14' max.

L Width 12' max.

M Height 8' min. clearance



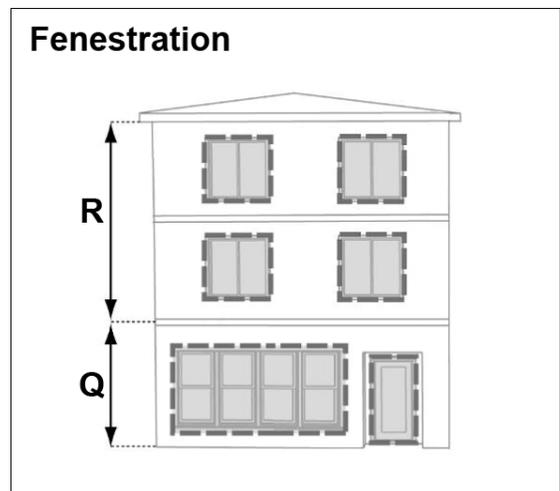
Fenestration

Q Street Level 60% min. retail

50% min. other commercial

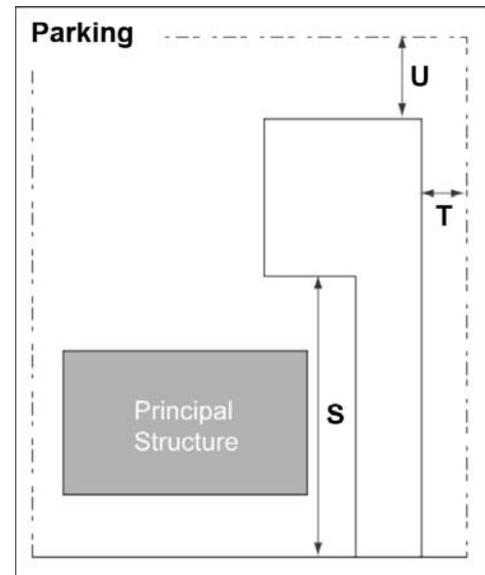
30% min. residential

R Upper Floors 30% min. – 70% max.



Parking

S	Front Setback	15' min. behind front facade for residential commercial: per site plan
T	Side Setback	2' min. 5' min. corner lots
U	Rear Yard Setback	3' min. from PL or from alley easement.



Accessory Structures

Setbacks	
Front	15' min. behind front facade
Side	2' min.
Rear	8' min adjacent to T3 5' min. from PL or alley easement.
Height	No taller than primary build.

Commercial and mixed-use buildings shall not have accessory structures

Streets

The Round Hill development will be served by a variety of street types, each designed to accommodate expected traffic volumes and compliment planned adjacent land uses. Individual Streetscape Section designs specify the location and size of travel lanes, on-street parking areas, landscaping zones, and sidewalks, and locate street trees, crosswalks, and other streetscape features.

The following Streetscape Sections apply, in whole or in part, to development in Round Hill's T4 zone:

- Major Village Street
- Minor Village Street
- Minor Village Street – Mixed Use
- Private Residential Alley

The applicable streetscape sections is presented in the adopted Street Map Diagram (refer to Exhibit 4.2 in Section 4 of this ordinance).

Streetscape Standards

The streetscape standards herein below are provided to ensure the coherence of streets within the T4-General Transect zone. In addition to other sections of this ordinance, these standards serve to assist building owners and operators with the understanding of the relationship between the street and privately-owned lots. These standards are intended to establish an environment that encourages and facilitates pedestrian activity. The streetscape standards for the T4-General Transect zone are prescribed below, provided that the requirements of this subsection shall apply to a subdivision and/or development subject to the requirement for a site development plan as well as to lot development plans for individual detached and duplex dwellings. Variations from these standards may be approved by the Planning Commission upon submission of a statement of justification and the presentation of satisfactory alternatives by the applicant.

1. Street Trees:

- (a) A street tree plan shall be submitted with any public right of way infrastructure plan and plats.
- (b) Street trees shall be installed by the developer along both sides of all streets within the T4 Transect zone.
- (c) Street trees shall be spaced at regular intervals of not fewer than 30 and not greater than 50 feet as measured parallel to the centerline of the street.
- (d) Street trees shall be limbed up so as to not interfere with pedestrian or automobile travel. A minimum vertical clearance of 7 feet shall be provided above the sidewalk and 14 feet above the travel lanes of the street.

2. Sidewalks and Pedestrian Facilities:

- (a) Sidewalks shall be installed by the developer along both sides of all public streets within the T4-General Transect zone. Where the T4-General Transect street section is contiguous with a T2-Open Space Transect, a system of internal pedestrian trails with the T2-Transect may be substituted for the required sidewalk on the T2 transect side of the street, subject to review by the Community Development Director and approval by the Planning Commission.
- (b) Sidewalks shall not be fewer than 5 feet in width when located within a public right of way and shall otherwise be installed in accord with the approved street section. Sidewalks shall otherwise be designed and constructed to meet all applicable handicap access regulations.

(c) Sidewalks in excess of 8 feet in width may be approved for dedication within the public right of way at the discretion of the Town and VDOT upon review of the final site plan and subdivision plat. Where sidewalks (or similarly paved surfaces intended for common usage) in excess of 8' in width are intended to accomplish the streetscape and civic space objectives of the applicant, the additional width shall be incorporated into the lot. Where deemed necessary by the Planning Commission, a private access easement dedicated to public use may be required to accomplish the unified streetscape and civic space goals.

(d) Consistency of paving design shall be required within the development project.

3. Alleys: The private access easement for any alley shall be not fewer than 16 feet and not more than 24 feet in width. Alley easements shall be platted and recorded prior to the sale of any lot that would be served by the alley. The oversight and maintenance of alleys shall be the responsibility of a property owners' association that shall be established prior to the sale of the first lot within the section of development in which the alley is to be located.

4. Common Civic and Open Space within Transect Zone:

(a) A minimum of one tot lot or playground shall be developed on a separate lot reserved and maintained for community use within each transect zone. (*See "Parks, Open Space, and Recreation Areas" Exhibit 4.3 in Section 4*).

(b) At least five percent (5%) of the total land area included as part of an individual development project involving the construction of forty (40) or more townhouse or multifamily dwelling units shall be preserved as common open space within the transect. This provision shall not apply to individual single-family, duplex, mixed-use, or live-work dwellings. (*See Recreational Facilities Standards sub-section.*)

5. Private Property Lighting: Site lighting shall be located and shall be of a design and height so as to illuminate only the interior of the property. A plan for the outdoor lighting design for a lot or project within the T4-General Transect zone shall be included with the site plan or lot development plan.

6. Private Property Landscaping: Landscaping on individual lots and projects shall be compatible with the street tree planting plan on public streets. A plan for the private lot or project landscape design for a lot or project within the T4-General Transect zone shall be included with the site plan or lot development plan, and shall be compatible with all Town of Orange landscape ordinances.

7. Street Lighting:

(a) A street lighting plan shall be included with the submission of public improvements plans for all streets within the Round Hill / UNO TND.

(b) Street lighting shall be installed by the developer concurrently with the development of the public right of way. Lighting shall be designed and installed to be fully shielded (i.e. full cutoff) and shall have a maximum lamp wattage consistent with the

requirements of the Town of Orange Public Works Manual or as otherwise approved during the site plan review process.

- (c) Street lights shall be installed along both sides of all streets.
- (d) Street lights shall be installed at least 12 feet and not more than 20 feet in height above grade and at regular intervals of not greater than 200 feet on center measured parallel to the centerline of the street.
- (e) In order to minimize light pollution, light should be directed downward to the immediate area being illuminated and away from any living quarters.
- (f) Floodlights or directional lights with maximum 100-watt metal halide bulbs may be used to illuminate alleys, parking garages and maintenance areas, but shall be shielded or aimed in such a way that they do not direct light onto other lots, the street, or out of the T4 General Transect.
- (g) Lights shall not be placed on the ground such that a beam of light is directed upward to illuminate the building wall(s). This provision shall not apply to floodlighting used to illuminate flags.
- (h) Site lighting shall be located and shall be of a design and height so as to illuminate only the interior of the property.
- (i) No flashing, traveling, animated, or intermittent lighting shall be visible from the exterior of any building whether such lighting is of temporary or long-term duration. This provision shall not apply to seasonal (i.e. holiday) lighting and/or displays.

Street Furniture

Streetscape furniture standards are provided to ensure the coherence of streets within the T4 General Transect. The standards serve to assist commercial, mixed-use, and high density residential building owners and operators with understanding the relationship between the street and their privately-owned lots. The street and building façade receives equal attention to the rest of the building. The desired aesthetic along the public right of way, civic spaces and the private area in front of commercial, mixed-use, and high density residential buildings is defined by streetscape elements, such as brick pavers, benches, lighting and waste-bins, street trees and public art. These standards, in concert with the Streetscape Standards, establish an environment that encourages and facilitates pedestrian activity.

1. Benches with backs and arm rests shall be provided at regular intervals not exceeding 250 feet along the street frontage, on average, measured parallel to the street beginning at the intersection of any two streets. The Victor Stanley Steelsites Model No. R-B 28 or equivalent model approved by the Community Development Director shall be used within the T4-General Transect.

2. At a minimum, one (1) waste bin shall be provided at each block corner with the site improvements for (a) non-residential, mixed-use, or multifamily building use or (b) a parks, recreation or open space lot, with placement subject to final design determinations. The Victor Stanley Steelsites Model No. NSDC-36 or equivalent model approved with the final site plan and subdivision plat shall be used within the T4 Transect.
3. Bicycle racks having a minimum two-bike capacity shall be installed along both sides of the street (at no more than 500-foot intervals, measured parallel to the street beginning at the intersection of any two streets) with the site improvements for (a) non-residential, mixed-use or multifamily buildings use, or (b) a parks, recreation or open space lot. The standard bike rack is an inverted “U” in galvanized steel with a baked-on black paint finish or as may be otherwise approved by the Planning Commission.
4. The placement, model and type of all street furniture and fixtures shall be of a design, placement, and construction specification subject to final site plan approval.



Victor Stanley Steelsites Model No. NSDC-36



Victor Stanley Steelsites Model No. R-B 28

Additional Transect Development Standards

1. Minimum 30 feet frontage for single family residential detached dwellings, where permitted, provided that cul-de-sac lots and “flag” (or “stem”) lots with less public street frontage may be approved on an individual basis by the Community Development Director. Public frontage requirements are not regulated for townhouse and duplex, but lot frontage geometry is subject to site plan review and approval. The total number of flag lots within the T4 Transect shall not exceed five percent (5%) of the total number of approved lots within the transect. A flag lot shall be limited to providing access to and accommodating a single primary dwelling unit. The entirety of the frontage and drive aisle portion of a flag lot shall be under individual ownership, with its geometric configuration integral to that of the platted lot and building that it serves.
2. A residential lot development plan shall be required with zoning permit applications for single family detached and duplex dwelling units. The zoning permit shall be issued upon approval of the lot development plan by the Community Development Director. Townhouses, multifamily residences and other uses shall be subject to site plan and subdivision plat approval. See Section 6: Residential Lot Development Plan Requirements.
3. Lot access: All single family detached residential lots which contain a residential dwelling or other permitted main building shall abut a public street.
4. Corner Lots: Of the two (2) sides of a corner lot, the front lot line shall be deemed to be the shorter of the two sides adjoining the street.
5. Street Terminus and Cul-de-sacs: Any temporary, dead-end street must terminate in a hammerhead or other alternative turnaround design located within a dedicated public right of way, provided, however, that a permanent cul-de-sac may be permitted.
6. Roof Pitch: Not regulated.
7. Accessory Structures: The footprint of an accessory structure shall not exceed 50 percent (50%) of the footprint of the principal dwelling up to a maximum area of 1,000 square feet. The height of the accessory structure shall not exceed the height of the principal dwelling. All accessory structures must be set back from the front lot line far enough so that at least 50 percent (50%) of the mass or depth of the principal structure is closer to the front lot line than is any portion of the accessory structure.

8. Calculation of Minimum and Maximum Residential Dwelling Count:

- MXPRD:** Maximum permitted residential dwellings (*in dwelling units*)
- MXPD:** Maximum permitted density in transect (*in dwelling units per acre*)
- MNPRD:** Minimum permitted residential dwellings (*in dwelling units*)
- MNPD:** Minimum permitted density in transect (*in dwelling units per acre*)
- TGLA:** Transect gross land area (*in acres*)
- TANR:** Transect area allocated to non-residential and mixed-use (*in acres*)

The maximum number of dwelling units permitted within the T4 Transect is calculated as follows:

$$\begin{aligned} & \textbf{Maximum Permitted Residential Dwellings} = \\ & \textbf{(Maximum Permitted Density in T4 Transect, in dwelling units per acre) X} \\ & \textbf{[(Total T4 Transect Gross Land Area, in acres) -} \\ & \textbf{(T4 Transect Area Allocated to Non-Residential and Mixed-Use, in acres)]} \end{aligned}$$

Abbreviated formula:

$$\textbf{MXPRD} = \textbf{MXPD X (TGLA - TANR)}$$

The minimum number of dwelling units permitted within the T4 Transect is calculated as follows:

$$\begin{aligned} & \textbf{Minimum Permitted Residential Dwellings} = \\ & \textbf{(Minimum Permitted Density in T4 Transect, in dwelling units per acre) X} \\ & \textbf{[(Total T4 Transect Gross Land Area, in acres) -} \\ & \textbf{(T4 Transect Area Allocated to Non-Residential and Mixed-Use, in acres)]} \end{aligned}$$

Abbreviated formula:

$$\textbf{MNPRD} = \textbf{MNPD X (TGLA - TANR)}$$

9. Transfer of Maximum Residential Dwelling Units Between Transects:

Up to five percent (5%) of the approved dwelling units within another transect may be transferred to the subject transect upon review and approval by the Planning Commission and, further, provided that (a) the maximum allowable number of units in the transferring transect (calculated by the formula in 8. hereinabove) shall be reduced by the number of units transferred, and (b) the increase in the total number of units in the receiving transect shall not exceed five percent (5%) of the number of units originally approved for the transect.

In transects where transference of dwelling units has been approved by the Town, the each lot development plan, site plan, and subdivision plat submitted to the Town shall include the following documentation:

- (a) a calculation of the cumulative dwelling units previously approved within the T4 Transect,
- (b) the cumulative number of dwelling units that have been applied to the individual project from the identified transferring transect, and
- (c) the cumulative number of of dwelling units that have been reduced in the transferring transect and the maximum number of permitted dwellings remaining in that transect.

Once the maximum allowable residential dwelling units within the transect have been approved for construction by the Town, no additional dwelling units may be approved to be constructed within the transect, provided that any subsequent modification to the maximum number of dwelling units shall be subject to a formal amendment to the Round Hill / UNO-TND Code of Development and the transect zoning regulations herein.

10. Calculation of Minimum and Maximum Building Floor Area for Non-Residential and Mixed-Used Buildings:

- MXPFA:** Maximum floor area of non-residential and mixed use buildings
(in square feet gross floor area, sfgfa)
- MXPFAR:** Maximum permitted floor area ratio, FAR *(a unit-less ratio)*
- MNPFA:** Minimum floor area of non-residential and mixed use buildings
(in square feet gross floor area, sfgfa)
- MNPFAR:** Minimum permitted floor area ratio, FAR *(a unit-less ratio)*
- TGLA:** Transect gross land area *(area in square feet)*
- TAR:** Transect area allocated to residential areas *(area in square feet)*

The maximum gross floor area for non-residential and mixed-use buildings permitted within the T4 Transect is calculated as follows:

$$\begin{aligned} & \textbf{Maximum Gross Floor Area for Non-Residential and Mixed-Use Buildings} = \\ & \textbf{(Maximum Permitted Floor Area Ratio (FAR) in T4 Transect) X} \\ & \textbf{[(Total T4 Transect Gross Land Area, in square feet) -} \\ & \textbf{(T4 Transect Area Allocated to Residential Areas, in square feet)]} \end{aligned}$$

Abbreviated formula:

$$\text{MXPFA} = \text{MXPFA} \text{ X (TGLA - TAR)}$$

The maximum gross floor area for non-residential and mixed-use buildings permitted within the T4 Transect is calculated as follows:

$$\begin{aligned} & \textbf{Minimum Gross Floor Area for Non-Residential and Mixed-Use Buildings} = \\ & \textbf{(Minimum Permitted Floor Area Ratio (FAR) in T4 Transect) X} \\ & \textbf{[(Total T4 Transect Gross Land Area, in square feet) -} \\ & \textbf{(T4 Transect Area Allocated to Residential Areas, in square feet)]} \end{aligned}$$

Abbreviated formula:

$$\text{MNPFA} = \text{MNPFA} \text{ X (TGLA - TAR)}$$

Once the maximum allowable gross floor area for non-residential and mixed-use within the transect has been approved for construction by the Town, no non-residential and mixed-use floor area may be approved to be constructed within the transect, provided that any subsequent modification to the maximum gross floor area shall be subject to a formal amendment to the Round Hill / UNO-TND Code of Development and the transect zoning regulations herein.

See [Appendix A](#) for an illustrative example for the procedure to calculate minimum and maximum permitted yield and mix of residential and non-residential/mixed areas in the T4 Transect Zone.

11. Residential units are permitted on the upper stories of mixed-use buildings above the ground floor non-residential footprint. The number and space allocated to dwelling units that are located in mixed-use buildings are regulated by the equation for minimum and maximum FAR (*see 10. above.*) and shall not be included in the count of maximum and minimum number of permitted residential dwelling units (*see 8. above.*)

12. Adjustment to Transect Area and Location: The approved location and acreage within the T4 Transect is as indicated on the adopted *Transect Map Diagram (Exhibit 4.1 in Section 4)*. Minor adjustments to the transect boundary and acreage may be approved by the Community Development Director subject to demonstration of need by the applicant based on final site plan, civil, engineering or geophysical determinations. Any variation to the size of the T4 Transect that increases or decreases the gross transect area by $\geq 10\%$ shall be subject to review and approval by the Planning Commission. Any change in transect area and acreage shall be documented by the applicant on a revised Transect Map Diagram. If approved, the revised Transect Map Diagram shall govern.

Recreational Facility Standards

Individual multi-family and townhouse projects of forty (40) dwelling units or more in the T4 Transect Zone shall provide improved recreational space for the use of residents, including facilities such as playgrounds, tot lots, civic space, tennis courts, swimming pools and clubhouses. A property owners' (or homeowners') association agreement shall be responsible for the ownership and maintenance of common property within the project.

Applicants for townhouse and multifamily projects with fewer than forty dwelling units shall demonstrate that the recreation and open space improvements in the planned and existing T2 Open Space transects are properly located and adequate to provide for the needs of project residents. If not adequately demonstrated, the Planning Commission, upon recommendation from the Community Development Director, may require recreational improvements within the proposed project.

All recreational and open space improvements shall meet the following minimum standards and supplementary requirements:

1. The size, type, and number of facilities provided shall be commensurate with the anticipated needs of the project's residents. Recreational improvements shall be coordinated with other open space and recreational improvements in the T2 Open Space Transect. Improvements to be located within the T2 Open Space Transect shall be taken into consideration in determining the relationship between supply and demand for said facilities.
2. In determining the extent of necessary facilities, the applicant shall meet with the Community Development Director prior to submission of final site plan in order to:
 - (a) coordinate improvements with the overall plan of open space and recreation improvements planned or existing in the T2 Open Space Transect Zone,
 - (b) coordinate improvements with the tot lot or recreation area planned or located within the transect in which the project is located, and

- (c) finalize the specific recreational improvements to be located within the proposed project. The applicant shall provide construction drawings and specifications for proposed facilities at the time of site plan and subdivision plat submittal.
3. Slopes should not exceed ten percent (10%) and the area for the placement of recreational facilities.
 4. The size and shape of each recreation area shall be adequate for the intended use.
 5. Groundcover shall be appropriate for the use and shall consist of turf grass or contained ground cover such as pine bark mulch, shredded tires, or pea gravel.
 6. Location, grading, and facility placement shall be compatible with adjoining uses, convenient to users, and suitable for parental supervision.
 7. All recreational equipment and facilities shall be designed for regular and extended use. Recreational equipment and facilities shall be maintained in a safe and attractive condition and replaced as necessary. Maintenance shall be the responsibility of the property owners' or homeowners' association.
 8. All required recreational facilities shall be completed or bonded for completion by the time of issuance of a Certificate of Occupancy for the units requiring the facilities.
 9. A property owners' (or homeowners') association agreement and articles of incorporation shall be established and recorded with each project. Such agreements shall address the ownership and maintenance responsibilities of recreational land and improvements.

Parking

1. The TND District encourages creative parking strategies that are aimed at balancing gross parking requirements for the project with opportunities for shared parking, on-street parking, phasing of parking, and coordination with environmentally responsive, low impact development techniques. This objective may be pursued with at the time application for individual site plan and subdivision plat approval (*see 9. and 10. herein below.*)
2. On-street parking is permitted in accord with individual street sections included in this Code of Development and as may be subsequently approved with final site plan and subdivision plats for individual projects.
3. In addition to on-street parking, off-street parking shall be provided on the individual lot subject to the zoning permit application. Parking on private property may be accessed via a driveway or drive aisle directly from the public street on which the lot fronts or via an approved alley located within the private access easement. Off-street parking access from a public street is subject to approval of a final site plan and subdivision plat.

4. Garages serving single family detached residential dwellings accessed from a public street frontage must be set back a minimum of 15' from the front building line as established for the residential unit by the SFD/Duplex Lot Development Plan.
5. Residences located in the upper levels of mixed-use buildings shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces. Not more than seventy-five percent (75%) of the required residential off-street spaces may be located within a garage or parking structure.
6. Townhouse and single family detached residences shall provide two (2) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage or parking structure.
7. Freestanding multi-family residences shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage or parking structure.
8. Commercial retail, office, and other non-residential uses within the transect shall conform to the parking requirements of the Town of Orange. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) The required commercial and other non-residential off-street spaces may be located within a parking structure, subject to final site plan and plat approval.
9. Applicants are encouraged to evaluate the benefits of shared parking and other parking reduction opportunities, where appropriate. The procedure for this is through the submission of a parking impact study. If the applicant elects to establish parking requirements based on a parking impact study, the study shall be subject to review and approval by the Planning Commission. The applicant shall prepare the parking impact study employing contemporary research and parking projection techniques, including studies prepared by the Urban Land Institute.

The parking impact study for a given project shall address: (a) planned and existing on-street parking supply, (b) shared-parking opportunities with other planned and existing residential and non-residential development projects, (c) mixed-use parking relationships, (d) location of project within the transect, (e) development phasing, (f) coordination with runoff reduction techniques, and (g) demonstration of total parking adequacy to meet the marketplace demands of the transect and project. The project-specific scope of the parking impact study shall be established at a meeting with the Community Development Director. The parking impact study shall be prepared by a qualified professional.

10. The parking impact study shall be submitted to the Community Development Director for review and approval at a minimum of thirty (30) days prior to submittal of the final site plan or subdivision plat for the project. Upon review, the Community Development

Director shall provide written comment to the applicant which shall be reflected in the final site plan.

Upon review of the parking impact study and the subsequent the review of the final site plan or subdivision plat, the Planning Commission may act to approve, deny, or amend with appropriate conditions the applicant's request for a reduction in the number of parking spaces or other aspects of parking requirements that would otherwise be required by the application of Town parking standards.

SFD/Duplex Residential Lot Development Plan

1. A lot development plan is a requirement of a zoning permit application for a duplex or single family detached residential unit(s). The lot development plan, prepared in accord with the requirements of the Section 6 of this ordinance, shall be submitted to the Town with the zoning permit application. Multiple units may be included on a lot development plan.
2. The lot development plan shall be (a) submitted with the zoning permit application, (c) demonstrate compatibility with the requirements herein above, (c) provide a cumulative total of the number of dwelling units previously approved for the transect, (d) exhibit consistency with the approved final site plan, transportation and utility plans, and subdivision plat for the block or transect infrastructure in which the building is proposed, and (e) incorporate the documentation and calculations required in *Additional Transect Development Standards, Paragraph. 9.*

Utility Requirements

1. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public water system.
2. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public sewer system.
3. All utilities that serve a structure shall be underground. Temporary construction shall be exempted from this requirement.

Special Administrative Standards: Modifications

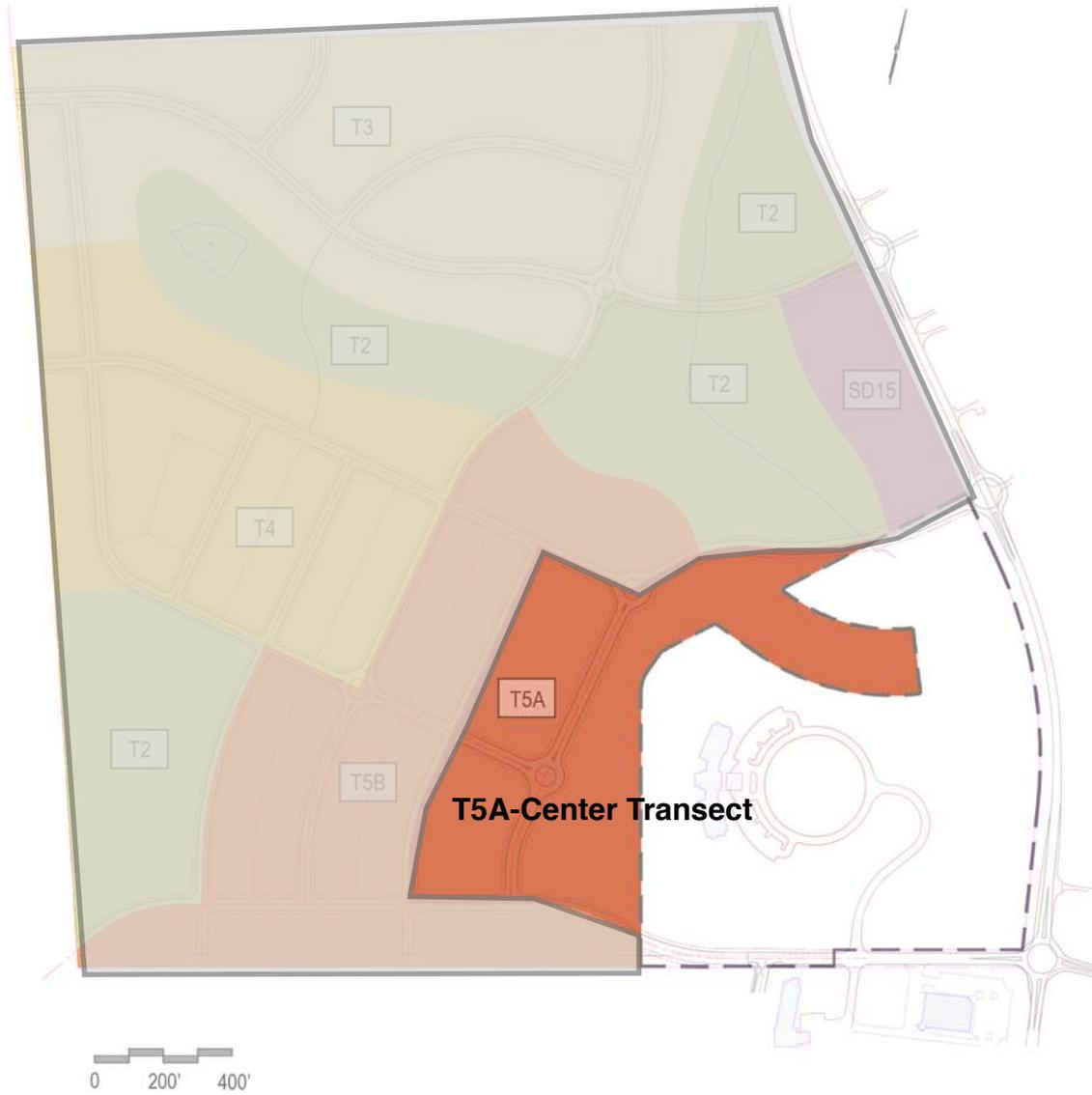
1. The Town recognizes a process by which to obtain "major modifications" and "minor modifications" to the TND District transect zone standards contained herein. Modifications to the transect zone standards that are deemed "major modifications" by the Community Development Director, in consultation with the Town Attorney, shall require the applicant to request a formal amendment to the approved zoning of the TND District. An amendment

to the zoning shall be processed by the Town in the same manner as an original zoning map amendment.

2. Modifications to the approved transect standards that are deemed “minor modifications” by the Community Development Director, in consultation with the Town Attorney, that are not otherwise delegated hereinabove to the Community Development Director may be approved by the Planning Commission upon submission of a statement of justification for the modification and the presentation of satisfactory alternatives by the applicant.
3. The Planning Commission shall consider the recommendation of the Community Development Director and other qualified sources in consideration of any minor modification from the approved transect standards hereinabove.
4. Requests for minor modifications from an approved transect standard shall be heard at a regularly scheduled meeting of the Planning Commission.

T5A-Center Transect Zone

17.06-acres



Round Hill/UNO-TND District

The Town of Orange

T5A Center Transect Zone

Intent

The purpose of T5A-Center Transect zone is to create a vibrant, pedestrian-oriented, commercial and mixed-use core (or town center) within the TND District, in keeping with the Town of Orange Comprehensive Plan. Similar in purpose and intent to the Town's Town Activity Center District, the regulations for the T5A-Center Transect zone promote a broad mix of uses, with a dominant focus on retail commercial and office uses. Supporting uses include high-density residential, live-work residential, and mixed-use projects.

The T5A-Center Transect standards accommodate both pedestrian and auto needs and create an attractive streetscape that is compatible with the surrounding neighborhoods. The transect plan should create an environment that serves contemporary market needs, recognizing the transect may include larger scale buildings than currently exist in the historic downtown. This zone is complemented by open space provided in the T2-Edge zones. The sub-sections herein below provide the regulatory standards for the transect zone. It is recognized that the developer may establish and impose more restrictive design guidelines.

Area and Density

The approved land area and the general location of the T5A-Center Transect Zone is depicted on the adopted Uptown North Orange TND Transect Map Diagram (Exhibit 4.1 in Section 4.) The minimum and maximum residential density, expressed in dwelling units per acre, is, as follows:

Minimum Residential Density: ***12.0 dwelling units per acre.***
Maximum Residential Density: ***24.0 dwelling units per acre.***

The minimum and maximum non-residential/mixed-use density, expressed in total non-residential and mixed-use floor area ratio (FAR), is, as follows:

Minimum Non-Residential/Mixed-Use Density: ***0.20 FAR***
Maximum Non-Residential/Mixed-Use Density: ***1.00 FAR***

(See "Additional Transect Development Standards" section for the method of calculation of the minimum and maximum permitted number of residential dwelling units and the minimum and maximum gross floor area of non-residential and mixed-used development within the T5A Transect Zone.)

Permitted Uses

The uses permitted by-right and permitted with a special use permit within the T5A-Center Transect Zone shall be in accordance with the Table of By-Right and Special Permitted Uses (Section 3).

Mix of Uses

A mix of residential and non-residential/mixed-uses shall be developed in the T5A Transect Zone. The following represents the permitted range in the allowable mix of uses, expressed as a percentage of the total T5A Transect Zone land area:

<i>Minimum Residential Percentage:</i>	0%
<i>Maximum Residential Percentage:</i>	70%
<i>Minimum Non-Residential/Mixed-Use Percentage:</i>	30%
<i>Maximum Non-Residential/Mixed-Use Percentage:</i>	100%

See Appendix A for an illustrative example of the calculation of minimum and maximum permitted mix of residential and non-residential/mixed uses in the T5A Transect Zone.

Development Standards

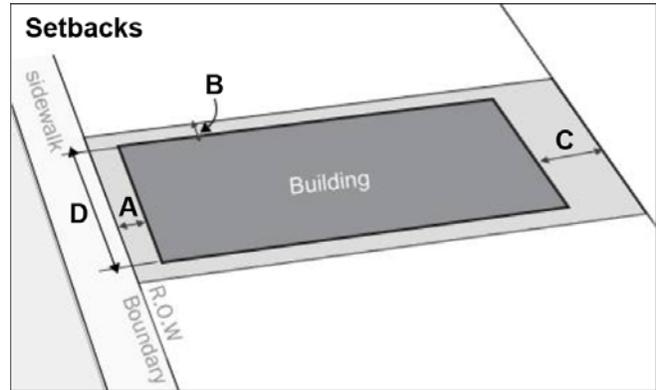
The general development standards in the T5A Transect for residential, non-residential and mixed-use buildings are regulated by the following tables and illustrations.

Building Lot Requirements

Setbacks

A	Street Front	<u>0' min. – 12' max. non-residential.</u> <u>12' min. - 20' max. residential</u>
B	Side	<u>0' min. – 3' max. non-residential.</u> <u>5' min. (35' residential combined max.)</u> <u>6' min. (18' max. residential corner lot)</u> <u>0' min. townhouse (5' min at corner lot)</u>
C	Rear	<u>0' min. non-res</u> <u>15' min. residential</u>
D	Facade buildout along frontage	<u>65% min. front street (50% residential)</u> <u>30% min. side street</u>

Maximum building footprint per non-residential establishment: 16,000 square feet gross floor area on first floor, provided that individual anchor commercial establishments (as defined) may be 60,000 square feet gross floor area. Establishments that exceed 60,000 square feet gross floor area shall require individual Special Use Permit approval.



Lot Requirements

Lot Area (See Table 7.1 for lot type & allocation guidelines)

Single family	3600 sf. min.
Townhouse	1500 sf min.
Multi family	per site plan
Non-residential	per site plan

Lot Width

Single family	36' min.
Town house	16' min.
Multi family	per site plan
Non-residential	per site plan

Lot Depth

Townhouse	75' min.
Single Family	85' min.
Multifamily	per site plan
Non-residential	per site plan

Lot Coverage

100% max. non-residential
90% max. town/multifamily
70% max single family

Block Length

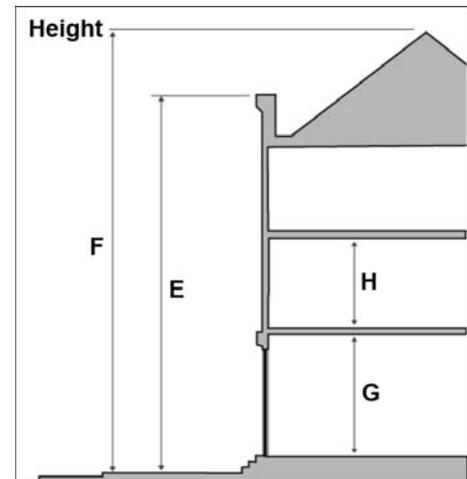
600' max.

Lot Access

Each lot must front a public street

Building Height

E	Building Height to eave	50' max. (2-5 stories) 40' max. (2-4 stories) res.
F	Building Height overall	60' max. (2-5 stories) 50' max. (2-4 stories) res.
G	First floor ceiling height	14' min. retail 12' min. other non-residential 9' min. civic / live-work 8' min. residential
H	Upper floor ceiling height	8' min. clear



Residential Encroachment

Front Porch (unenclosed) max. 4' into minimum residential front setback
Chimney max. 1' into minimum residential side setback
(applicable to single family, townhouse, and live-work units.)

Frontage Types

Porch

I Depth 6' min. – 14' max.

Stoop

J Depth 5' min. – 12' max.

Awning

K Depth 12' max.

L Width 12' max.

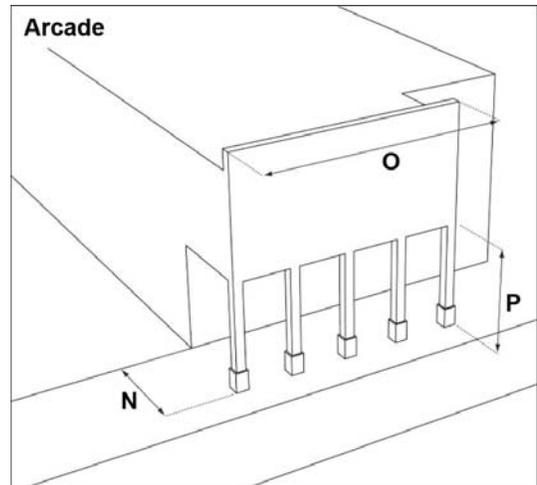
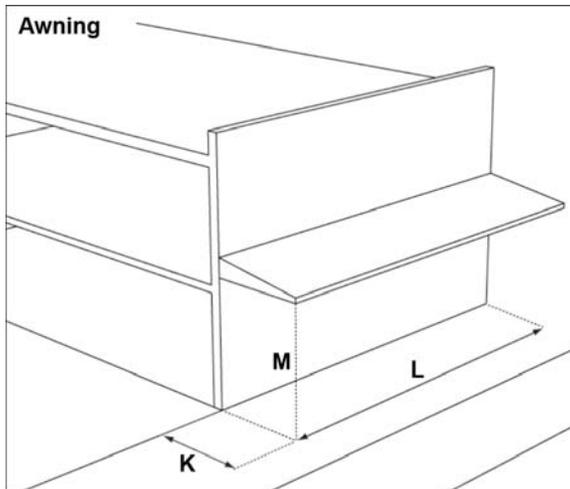
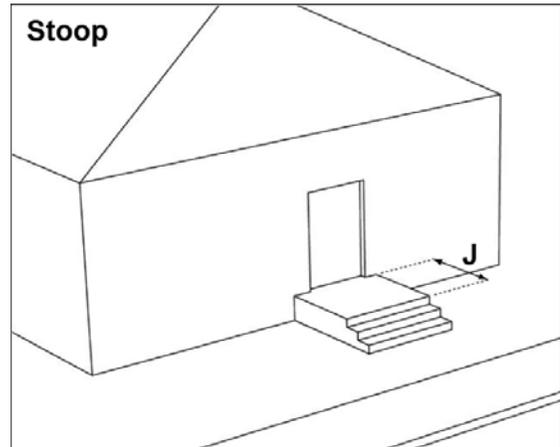
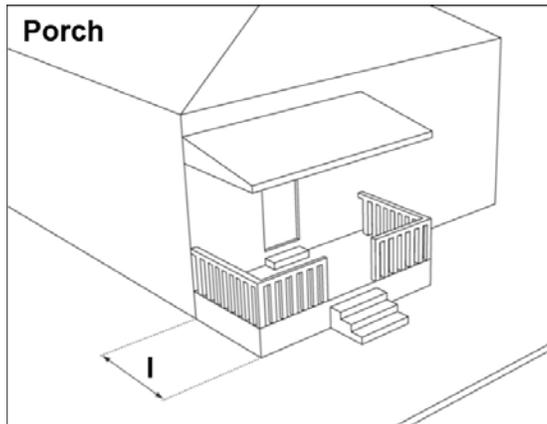
M Height 8' min. clear

Arcade

N Depth 8' min.

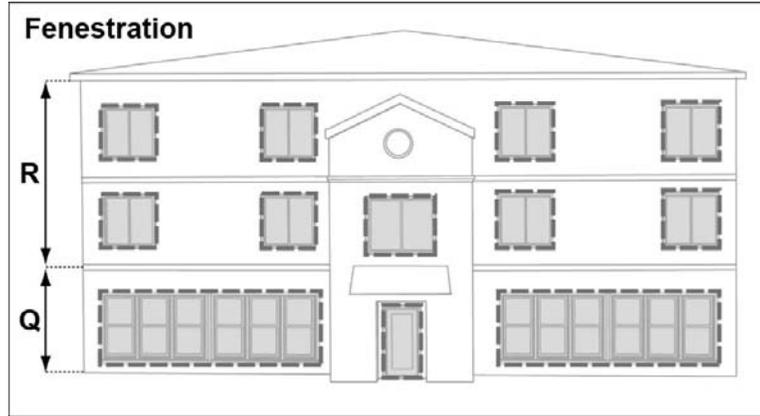
O Width 12' min.

P Height 10' min. clear



Fenestration

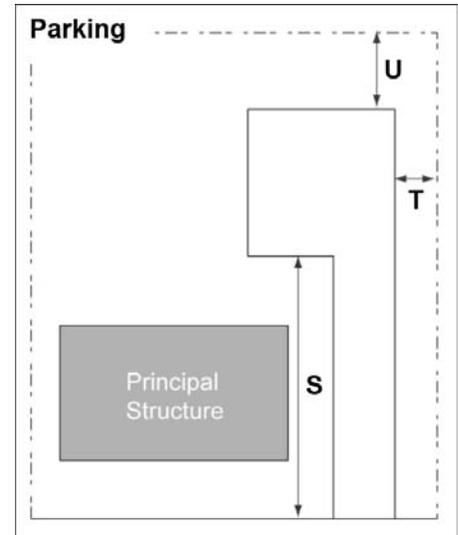
Q	Street Level	<u>40% min. retail</u> <u>20% min. other commercial</u> <u>30% min. residential</u>
R	Upper Floors	<u>15% min. – 40% max.</u>



Parking

S	Front Setback	<u>non-res. per site plan</u> <u>res.: 15' behind front façade</u>
T	Side Setback	<u>2' min.</u> <u>5' min. corner lots</u>
U	Rear Yard Setback	<u>10' min.</u>

Parking must be visually screened from side and rear lot lines either by the principal structure, or by 3'-6' opaque fence and/or vegetative screen.



Accessory Structures

Setbacks	
Front	<u>15' min. behind front facade</u>
Side	<u>2' min.</u>
Rear	<u>8' min adjacent to T3</u> <u>5' min. from PL or alley easement.</u>
Height	<u>No taller than primary build.</u>

Commercial and mixed-use buildings shall not have accessory structures

Streets

The Round Hill development will be served by a variety of street types, each designed to accommodate expected traffic volumes and compliment planned adjacent land uses. Individual Streetscape Section designs specify the location and size of travel lanes, on-street parking areas, landscaping zones, and sidewalks, and locate street trees, crosswalks, and other streetscape features.

The following Streetscape Sections apply, in whole or in part, to development in Round Hill's T5A zone:

- Major Village Street
- Major Village Street – Mixed Use
- Minor Village Street
- Minor Village Street – Mixed Use
- Radney Road
- Private Residential Alley

The applicable streetscape sections is presented in the adopted Street Map Diagram (refer to Exhibit 4.2 in Section 4 of this ordinance.)

Streetscape Standards

The streetscape standards herein below are provided to ensure the coherence of streets within the T5A-Center Transect zone. In addition to other sections of this ordinance, these standards serve to assist building owners and operators with the understanding of the relationship between the street and privately-owned lots. These standards are intended to establish an environment that encourages and facilitates pedestrian activity. The streetscape standards for the T5A-Center Transect zone are prescribed below, provided that the requirements of this subsection shall apply to a subdivision and/or development subject to the requirement for a site development plan as well as to lot development plans for individual detached and duplex dwellings. Variations from these standards may be approved by the Planning Commission upon submission of a statement of justification and the presentation of satisfactory alternatives by the applicant.

1. Street Trees:

- (a) A street tree plan shall be submitted with any public right of way infrastructure plan and plats.
- (b) Street trees shall be installed by the developer along both sides of all streets within the T5A-Center Transect zone.
- (c) Street trees shall be spaced at regular intervals of not fewer than 30 and not greater than 50 feet as measured parallel to the centerline of the street.

(d) Street trees shall be limbed up so as to not interfere with pedestrian or automobile travel. A minimum vertical clearance of 7 feet shall be provided above the sidewalk and 14 feet above the travel lanes of the street.

(e) When street trees are to be located in the sidewalk or other paved civic area, tree grates of a design, material, and location subject to final site plan approval shall be installed around street tree pit areas to prevent pedestrian damage to planting materials.

2. Sidewalks and Pedestrian Facilities:

(a) Sidewalks shall be installed by the developer along both sides of all public streets within the T5A-Center Transect zone. Where the T5A-Center Transect street section is contiguous with a T2-Open Space Transect, a system of internal pedestrian trails located interior to the T2-Transect may be substituted for the required sidewalk on the T2 transect side of the street, subject to review by the Community Development Director and approval by the Planning Commission.

(b) Sidewalks shall not be fewer than 5 feet in width when located within a public right of way and shall otherwise be installed in accord with the approved street section. Sidewalks shall otherwise be designed and constructed to meet all applicable handicap access regulations.

(c) Sidewalks in excess of 8 feet in width may be approved for dedication within the public right of way at the discretion of the Town and VDOT upon review of the final site plan and subdivision plat. Where sidewalks (or similarly paved surfaces intended for common usage) in excess of 8' in width are intended to accomplish the streetscape and civic space objectives of the applicant, the additional width shall be incorporated into the lot. Where deemed necessary by the Planning Commission, a private access easement dedicated to public use may be required to accomplish the unified streetscape and civic space goals.

(d) consistency of paving design shall be required within the development project.

3. Alleys: The private access easement for any alley shall be not fewer than 16 feet and not more than 24 feet in width. Alley easements shall be platted and recorded prior to the sale of any lot that would be served by the alley. The oversight and maintenance of alleys shall be the responsibility of a property owners' association that shall be established prior to the sale of the first lot within the section of development in which the alley is to be located.

4. Common Civic and Open Space within Transect Zone:

(a) A minimum of one tot lot or playground shall be developed on a separate lot reserved and maintained for community use within each transect zone. (*See "Parks, Open Space, and Recreation Areas" Exhibit 4.2 in Section 4*).

(b) At least five percent (5%) of the total land area included as part of an individual development project involving the construction of forty (40) or more townhouse or multifamily dwelling units shall be preserved as common open space within the transect.

This provision shall not apply to individual single-family, duplex, mixed-use, or live-work dwellings.

5. Private Property Lighting: Site lighting shall be located and shall be of a design and height so as to illuminate only the interior of the property. A plan for the outdoor lighting design for a lot or project within the T5A-Center Transect zone shall be included with the site plan or lot development plan.
6. Private Property Landscaping: Landscaping on individual lots and projects shall be compatible with the street tree planting plan on public streets. A plan for the private lot or project landscape design for a lot or project within the T5A-Center Transect zone shall be included with the site plan or lot development plan, and shall be compatible with all Town of Orange landscape ordinances.
7. Street Lighting:

(a) A street lighting plan shall be included with the submission of public improvements plans for all streets within the UNO-TND.

(b) Street lighting shall be installed by the developer concurrently with the development of the public right of way. Lighting shall be designed and installed to be fully shielded (i.e. full cutoff) and shall have a maximum lamp wattage consistent with the requirements of the Town of Orange Public Works Manual or as otherwise approved during the site plan review process.

(c) Street lights shall be installed along both sides of all streets. The Lumec Candela CAND1-CN1 with 100-watt metal halide bulbs, or equivalent model in compliance with the Town of Orange Public Works Manual.

(d) Street lights shall be installed at least 12 feet and not more than 20 feet in height above grade and at regular intervals of not greater than 200 feet on center measured parallel to the centerline of the street.

(e) In order to minimize light pollution, light should be directed downward to the immediate area being illuminated and away from any living quarters.



Lumec Candela CAND1-CN1

(f) Floodlights or directional lights with maximum 100-watt metal halide bulbs may be used to illuminate alleys, parking garages and maintenance areas, but shall be shielded or aimed in such a way that they do not direct light onto other lots, the street, or out of the parcel.

(g) Lights shall not be placed on the ground such that a beam of light is directed upward to illuminate the building wall(s). This provision shall not apply to floodlighting used to illuminate flags.

(h) Site lighting shall be located and shall be of a design and height so as to illuminate only the interior of the property.

(i) No flashing, traveling, animated, or intermittent lighting shall be visible from the exterior of any building whether such lighting is of temporary or long-term duration. This provision shall not apply to seasonal (i.e. holiday) lighting and/or displays.

Street Furniture

Streetscape furniture standards are provided to ensure the coherence of streets within the T5A-Center Transect. The standards serve to assist commercial, mixed-use, and high density residential building owners and operators with understanding the relationship between the street and their privately-owned lots. The street and building façade receives equal attention to the rest of the building. The desired aesthetic along the public right of way, civic spaces and the private area in front of commercial, mixed-use, and high density residential buildings is defined by streetscape elements, such as brick pavers, benches, lighting and waste-bins, street trees and public art. These standards, in concert with the Streetscape Standards, establish an environment that encourages and facilitates pedestrian activity.

1. Benches with backs and arm rests shall be provided at regular intervals not exceeding 250 feet along the street frontage, on average, measured parallel to the street beginning at the intersection of any two streets. The Victor Stanley Steelsites Model No. R-B 28 or equivalent model approved with the final site plan and plat shall be used within the T5A-Center Transect.
2. At a minimum, one (1) waste bin shall be provided at each block corner with the site improvements for (a) non-residential, mixed-use, or multifamily building use or (b) a parks, recreation or open space lot, with placement subject to final design determinations. The Victor Stanley Steelsites Model No. NSDC-36 or equivalent model approved with the final site plan and subdivision plat shall be used within the T4 Transect.
3. Bicycle racks having a minimum two-bike capacity shall be installed along both sides of the street (at no more than 500-foot intervals, measured parallel to the street beginning at the intersection of any two streets) with the site improvements for (a) non-residential, mixed-use or multifamily building use, or (b) a parks, recreation or open space lot. The standard bike rack is an inverted “U” in galvanized steel with a baked-on black paint finish or as may be otherwise approved by the Planning Commission.
4. The placement, model and type of all street furniture and fixtures shall be of a design, placement, and construction specification subject to final site plan approval.



Victor Stanley Steelsites Model No. NSDC-36



Victor Stanley Steelsites Model No. R-B 28

Additional Transect Development Standards

1. Minimum 30 feet frontage for single family residential detached dwellings, where permitted, provided that cul-de-sac lots and “flag” (or “stem”) lots with less public street frontage may be approved on an individual basis by the Community Development Director. Public frontage requirements are not regulated for townhouse and duplex, but lot frontage geometry is subject to site plan review and approval. The total number of flag lots within the T5A Transect shall not exceed five percent (5%) of the total number of approved lots within the transect. A flag lot shall be limited to providing access to and accommodating a single dwelling unit. The entirety of the frontage and drive aisle portion of a flag lot shall be under individual ownership with its geometric configuration integral to that of the platted lot and building that it serves.
2. A residential lot development plan shall be required with zoning permit applications for single family detached and duplex dwelling units. The zoning permit shall be issued upon approval of the lot development plan by the Community Development Director. Townhouses, multifamily residences and other uses shall be subject to site plan and subdivision plat approval. See Section 6 *Residential Lot Development Plan Requirements*.
3. Lot access: All single family detached residential lots which contain a residential dwelling or other permitted main building shall abut a public street.
4. Corner Lots: Of the two (2) sides of a corner lot, the front lot line shall be deemed to be the shorter of the two sides adjoining the street.

5. Street Terminus and Cul-de-sacs: Any temporary, dead-end street must terminate in a hammerhead or other alternative turnaround design located within a dedicated public right of way, provided, however, that a permanent cul-de-sac may be permitted.
6. Roof Pitch: Not regulated.
7. Accessory Structures: The footprint of an accessory structure shall not exceed 50 percent (50%) of the footprint of the principal dwelling up to a maximum area of 1,000 square feet. The height of the accessory structure shall not exceed the height of the principal dwelling. All accessory structures must be set back from the front lot line far enough so that at least 50 percent (50%) of the mass or depth of the principal structure is closer to the front lot line than is any portion of the accessory structure. Accessory structures shall not be allowed for non-residential uses.
8. Calculation of Minimum and Maximum Residential Dwelling Count:

- MXPRD:** Maximum permitted residential dwellings (*in dwelling units*)
- MXPD:** Maximum permitted density in transect (*in dwelling units per acre*)
- MNPRD:** Minimum permitted residential dwellings (*in dwelling units*)
- MNPD:** Minimum permitted density in transect (*in dwelling units per acre*)
- TGLA:** Transect gross land area (*in acres*)
- TANR:** Transect area allocated to non-residential and mixed-use (*in acres*)

The maximum number of residential dwelling units permitted within the T5A Transect is calculated as follows:

$$\begin{aligned} & \textbf{Maximum Permitted Residential Dwellings} = \\ & (\textbf{Maximum Permitted Density in T5A Transect, in dwelling units per acre}) \times \\ & [(\textbf{Total T5A Transect Gross Land Area, in acres}) - \\ & (\textbf{T5A Transect Area Allocated to Non-Residential and Mixed-Use, in acres})] \end{aligned}$$

Abbreviated formula:

$$\textbf{MXPRD} = \textbf{MXPD} \times (\textbf{TGLA} - \textbf{TANR})$$

The minimum number of dwelling units permitted within the T5A Transect is calculated as follows:

$$\begin{aligned} & \textbf{Minimum Permitted Residential Dwellings} = \\ & \textbf{(Minimum Permitted Density in T5A Transect, in dwelling units per acre) X} \\ & \textbf{[(Total T5A Transect Gross Land Area, in acres) -} \\ & \textbf{(T5A Transect Area Allocated to Non-Residential and Mixed-Use, in acres)]} \end{aligned}$$

Abbreviated formula:

$$\textbf{MNPRD} = \textbf{MNPD X (TGLA - TANR)}$$

9. Transfer of Maximum Residential Dwelling Units Between Transects:

Up to five percent (5%) of the approved dwelling units within another transect may be transferred to the subject transect upon review and approval by the Planning Commission and, further, provided that (a) the maximum allowable number of units in the transferring transect (calculated by the formula in 8. hereinabove) shall be reduced by the number of units transferred, and (b) the increase in the total number of units in the receiving transect shall not exceed five percent (5%) of the number of units originally approved for the transect.

In transects where transference of dwelling units has been approved by the Town, the each lot development plan, site plan, and subdivision plat submitted to the Town shall include the following documentation:

- (a) a calculation of the cumulative dwelling units previously approved within the T5A Transect,
- (b) the cumulative number of dwelling units that have been applied to the individual project from the identified transferring transect, and
- (c) the cumulative number of dwelling units that have been reduced in the transferring transect and the maximum number of permitted dwellings remaining in that transect.

Once the maximum allowable residential dwelling units within the transect have been approved for construction by the Town, no additional dwelling units may be approved to be constructed within the transect, provided that any subsequent modification to the maximum number of dwelling units shall be subject to a formal amendment to the Round Hill / UNO-TND Code of Development and the transect zoning regulations herein.

10. Calculation of Minimum and Maximum Building Floor Area for Non-Residential and Mixed-Used Buildings:

- MXPFA:** Maximum floor area of non-residential and mixed use buildings
(in square feet gross floor area, sfgfa)
- MXPFAR:** Maximum permitted floor area ratio, FAR *(a unit-less ratio)*
- MNPFA:** Minimum floor area of non-residential and mixed use buildings
(in square feet gross floor area, sfgfa)
- MNPFAR:** Minimum permitted floor area ratio, FAR *(a unit-less ratio)*
- TGLA:** Transect gross land area *(in square feet)*
- TAR:** Transect area allocated to residential areas *(in square feet)*

The maximum gross floor area for non-residential and mixed-use buildings permitted within the T5A Transect is calculated as follows:

$$\begin{aligned} &\textbf{Maximum Gross Floor Area for Non-Residential and Mixed-Use Buildings} = \\ &\quad \text{(Maximum Permitted Floor Area Ratio (FAR) in T5A Transect) X} \\ &\quad \text{[(Total T5A Transect Gross Land Area, in square feet) -} \\ &\quad \text{(T5A Transect Area Allocated to Residential Areas, in square feet)]} \end{aligned}$$

Abbreviated formula:

$$\text{MXPFA} = \text{MXPFAR X (TGLA - TAR)}$$

The minimum gross floor area for non-residential and mixed-use buildings permitted within the T5A Transect is calculated as follows:

$$\begin{aligned} &\textbf{Minimum Gross Floor Area for Non-Residential and Mixed-Use Buildings} = \\ &\quad \text{(Minimum Permitted Floor Area Ratio (FAR) in T5A Transect) X} \\ &\quad \text{[(Total T5A Transect Gross Land Area, in square feet) -} \\ &\quad \text{(T5A Transect Area Allocated to Residential Areas, in square feet)]} \end{aligned}$$

Abbreviated formula:

$$\text{MNPFA} = \text{MNPFAR X (TGLA - TAR)}$$

Once the maximum allowable gross floor area for non-residential and mixed-use within the transect has been approved for construction by the Town, no non-residential and mixed-use floor area may be approved to be constructed within the transect, provided that any subsequent modification to the maximum gross floor area shall be subject to a formal amendment to the Round Hill / UNO-TND Code of Development and the transect zoning regulations herein.

See Appendix A for an illustrative example for the procedure to calculate minimum and maximum permitted yield and mix of residential and non-residential/mixed uses in the T5A Transect Zone.

11. Residential units are permitted on the upper stories of mixed-use buildings above the ground floor non-residential footprint. The number and space allocated to dwelling units that are located in mixed-use buildings are regulated by the equation for minimum and maximum FAR (*see 10. above.*) and shall not be included in the count of maximum and minimum number of permitted residential dwelling units (*see 8. above.*)
12. Adjustment to Transect Area and Location: The approved location and acreage within the T5A Transect is as indicated on the adopted Uptown North Orange TND Transect Map Diagram (Exhibit 4.1 in Section 4). Minor adjustments to the transect boundary and acreage may be approved by the Community Development Director subject to demonstration of need by the applicant based on final site plan, civil, engineering or geophysical determinations. Any variation to the size of the T5A Transect that increases or decreases the gross transect area by $\geq 10\%$ shall be subject to review and approval by the Planning Commission. Any change in transect area and acreage shall be documented by the applicant on a revised Transect Map Diagram. If approved, the revised Transect Map Diagram shall govern.

Recreational Facility Standards

Individual multi-family and townhouse projects of forty (40) dwelling units or more shall provide improved recreational space for the use of residents, including facilities such as playgrounds, tot lots, civic space, tennis courts, swimming pools and clubhouses. A property owners' (or homeowners') association agreement shall be responsible for the ownership and maintenance of common property within the project.

Applicants for townhouse and multifamily projects with fewer than forty dwelling units shall demonstrate that the recreation and open space improvements in the planned and existing T2 Open Space transects are properly located and adequate to provide for the needs of project residents. If not adequately demonstrated, the Planning Commission, upon recommendation from the Community Development Director, may require recreational improvements within the proposed project.

All recreational and open space improvements shall meet the following minimum standards and supplementary requirements:

1. The size, type, and number of facilities provided shall be commensurate with the anticipated needs of the project's residents. Recreational improvements shall be coordinated with other open space and recreational improvements in the T2 Open Space Transect. Improvements to be located within the T2 Open Space Transect shall be taken into consideration in determining the relationship between supply and demand for said facilities.
2. In determining the extent of necessary facilities, the applicant shall meet with the Community Development Director prior to submission of final site plan in order to:
 - (a) coordinate improvements with the overall plan of open space and recreation improvements planned or existing in the T2 Open Space Transect Zone,
 - (b) coordinate improvements with the tot lot or recreation area planned or located within the transect in which the project is located, and
 - (c) finalize the specific recreational improvements to be located within the proposed project. The applicant shall provide construction drawings and specifications for proposed facilities at the time of site plan and subdivision plat submittal.
3. Slopes should not exceed ten percent (10%) and the area for the placement of recreational facilities.
4. The size and shape of each recreation area shall be adequate for the intended use.
5. Groundcover shall be appropriate for the use and shall consist of turf grass or contained ground cover such as pine bark mulch, shredded tires, or pea gravel.
6. Location, grading, and facility placement shall be compatible with adjoining uses, convenient to users, and suitable for parental supervision.
7. All recreational equipment and facilities shall be designed for regular and extended use. Recreational equipment and facilities shall be maintained in a safe and attractive condition and replaced as necessary. Maintenance shall be the responsibility of the property owners' or homeowners' association.
8. All required recreational facilities shall be completed or bonded for completion by the time of issuance of a Certificate of Occupancy for the units requiring the facilities.
9. A property owners' (or homeowners') association agreement and articles of incorporation shall be established and recorded with each project. Such agreements shall address the ownership and maintenance responsibilities of recreational land and improvements.

Parking

1. The TND District encourages creative parking strategies that are aimed at balancing gross parking requirements for the project with opportunities for shared parking, on-street parking, phasing of parking, and coordination with environmentally responsive, low impact development techniques. This objective may be pursued at the time application for individual site plan and subdivision plat approval (*see 9. and 10. herein below.*)
2. On-street parking is permitted in accord with individual street sections included in this Code of Development and as may be subsequently approved with final site plan and subdivision plats for individual projects.
3. In addition to on-street parking, off-street parking shall be provided on the individual lot subject to the zoning permit application. Parking on private property may be accessed via a driveway or drive aisle directly from the public street on which the lot fronts or via an approved alley located within the private access easement. Off-street parking access from a public street is subject to approval of a final site plan and subdivision plat.
4. Garages serving single family detached residential dwellings accessed from a public street frontage must be set back a minimum of 15' from the front building line as established for the residential unit by the SFD/Duplex Lot Development Plan.
5. Residences located in the upper levels of mixed-use buildings shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces. Not more than seventy-five percent (75%) of the required residential off-street spaces may be located within a garage or parking structure.
6. Townhouse and single family detached residences shall provide two (2) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage or parking structure.
7. Freestanding multi-family residences shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage or parking structure.
8. Commercial retail, office, and other non-residential uses within the transect shall conform to the parking requirements of the Town of Orange. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) The required commercial and other non-residential off-street spaces may be located within a parking structure, subject to final site plan and plat approval.

9. Applicants are encouraged to evaluate the benefits of shared parking and other parking reduction opportunities, where appropriate. The procedure for this is through the submission of a parking impact study. If the applicant elects to establish parking requirements based on a parking impact study, the study shall be subject to review and approval by the Planning Commission. The applicant shall prepare the parking impact study employing contemporary research and parking projection techniques, including studies prepared by the Urban Land Institute.

The parking impact study for a given project shall address: (a) planned and existing on-street parking supply, (b) shared-parking opportunities with other planned and existing residential and non-residential development projects, (c) mixed-use parking relationships, (d) location of project within the transect, (e) development phasing, (f) coordination with runoff reduction techniques, and (g) demonstration of total parking adequacy to meet the marketplace demands of the transect and project. The project-specific scope of the parking impact study shall be established at a meeting with the Community Development Director. The parking impact study shall be prepared by a qualified professional.

10. The parking impact study shall be submitted to the Community Development Director for review and approval at a minimum of thirty (30) days prior to submittal of the final site plan or subdivision plat for the project. Upon review, the Community Development Director shall provide written comment to the applicant which shall be reflected in the final site plan.

Upon review of the parking impact study and the subsequent the review of the final site plan or subdivision plat, the Planning Commission may act to approve, deny, or amend with appropriate conditions the applicant's request for a reduction in the number of parking spaces or other aspects of parking requirements that would otherwise be required by the application of Town parking standards.

SFD/Duplex Residential Lot Development Plan

1. A lot development plan is a requirement of a zoning permit application for a duplex or single family detached residential unit(s). The lot development plan, prepared in accord with the requirements of the Section 6 of this ordinance, shall be submitted to the Town with the zoning permit application. Multiple units may be included on a lot development plan.
2. The lot development plan shall be (a) submitted with the zoning permit application, (c) demonstrate compatibility with the requirements herein above, (c) provide a cumulative total of the number of dwelling units previously approved for the transect, (d) exhibit consistency with the approved final site plan, transportation and utility plans, and subdivision plat for the block or transect infrastructure in which the building is proposed, and (e) incorporate the documentation and calculations required in *Additional Transect Development Standards, Paragraph. 9.*

Utility Requirements

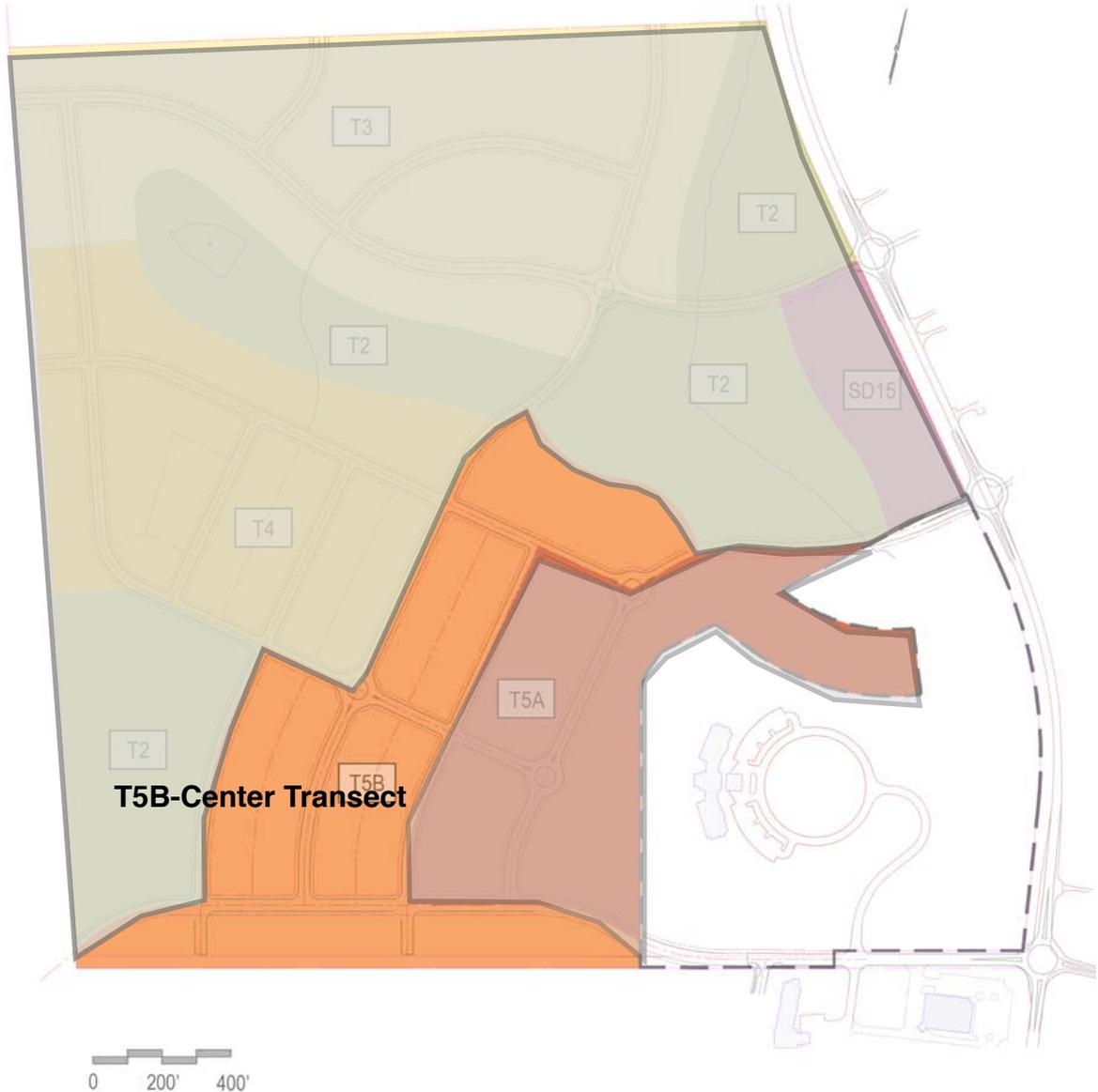
1. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public water system.
2. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public sewer system.
3. All utilities that serve a structure shall be underground. Temporary construction shall be exempted from this requirement.

Special Administrative Standards: Modifications

1. The Town recognizes a process by which to obtain “major modifications” and “minor modifications” to the TND District transect zone standards contained herein. Modifications to the transect zone standards that are deemed “major modifications” by the Community Development Director, in consultation with the Town Attorney, shall require the applicant to request a formal amendment to the approved zoning of the TND District. An amendment to the zoning shall be processed by the Town in the same manner as an original zoning map amendment.
2. Modifications to the approved transect standards that are deemed “minor modifications” by the Community Development Director, in consultation with the Town Attorney, that are not otherwise delegated hereinabove to the Community Development Director may be approved by the Planning Commission upon submission of a statement of justification for the modification and the presentation of satisfactory alternatives by the applicant.
3. The Planning Commission shall consider the recommendation of the Community Development Director and other qualified sources in consideration of any minor modification from the approved transect standards hereinabove.
4. Requests for minor modifications from an approved transect standard shall be heard at a regularly scheduled meeting of the Planning Commission.

T5B-Center Transect Zone

22.83-acres



Round Hill/UNO-TND District

The Town of Orange

T5B Center Transect Zone

Intent

The purpose of T5B-Center Transect zone is to support the contiguous T5A-Center Transect retail core transect with slightly lower density, pedestrian-oriented uses. The emphasis is on a mix of medium-density residential uses complemented by compatible neighborhood retail, restaurant, service, and office establishments to serve the needs of the Uptown North neighborhoods. The regulations for the T5B-Center Transect zone encourage a variety of residential uses, including townhouse, single family, live-work, multifamily, and retirement-oriented housing.

The T5B-Center Transect standards address both pedestrian and automobile-oriented needs, with the objective to create an attractive streetscape that is compatible with the more intensively developed T5A transect and the surrounding TND community. This zone is complemented by open space provided in the T2-Edge zones. The sub-sections herein below provide the regulatory standards for the transect zone. It is recognized that the developer may establish and impose more restrictive design guidelines.

Area and Density

The approved land area and the general location of the T5B-Center Transect Zone is depicted on the Transect Map Diagram (Exhibit 4.1 in Section 4). The minimum and maximum residential density, expressed in dwelling units per acre, is, as follows:

Minimum Residential Density: 9.0 dwelling units per acre.
Maximum Residential Density: 18.0 dwelling units per acre.

The minimum and maximum non-residential density, expressed in total non-residential and mixed-use floor area ratio (FAR), is, as follows:

Minimum Non-Residential/Mixed-Use Density: 0.20 FAR
Maximum Non-Residential/Mixed-Use Density: 0.75 FAR

(See “Additional Transect Development Standards” section for the method of calculation of the minimum and maximum permitted number of residential dwelling units and the minimum and maximum gross floor area of non-residential and mixed-used development within the T5B Transect Zone.)

Permitted Uses

The uses permitted by-right and permitted with a special use permit within the T5B-Center Transect Zone shall be in accordance with the Table of By-Right and Special Permitted Uses (Section 3).

Mix of Uses

A mix of residential and non-residential/mixed-uses shall be developed in the T5B Transect Zone. The following represents the permitted range in the allowable mix of uses, expressed as a percentage of the total T5B Transect Zone land area:

<i>Minimum Residential Percentage:</i>	20%
<i>Maximum Residential Percentage:</i>	80%
<i>Minimum Non-Residential/Mixed-Use Percentage:</i>	20%
<i>Maximum Non-Residential/Mixed-Use Percentage:</i>	80%

See Appendix A for an illustrative example of the calculation of minimum and maximum permitted mix of residential and non-residential/mixed uses in the T5B Transect Zone.

Development Standards

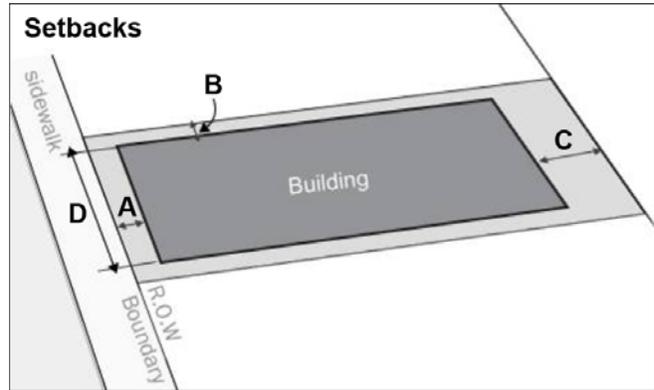
The general development standards in the T5A Transect for residential, non-residential and mixed-use buildings are regulated by the following tables and illustrations.

Building Requirements (non-residential and mixed-use buildings)

Setbacks

A	Street Front	0' min. – 20' max. non-residential.
		12' min. - 20' max. residential
B	Side	0' min. – 10' max. non-residential.
		3' min. (15' max. non-res. corner lot)
		5' min. (35' residential combined max.)
		6' min. – 18' max. residential corner lot
		0' min. town-house; (5' min at corner lot)
C	Rear	0' min. non-res
		12' min. residential
D	Facade buildout along frontage	50% min. front street
		25% min. side street

Maximum building footprint per non-residential establishment: 8000 square feet gross floor area on first floor, provided that establishments that exceed 8000 square feet gross floor area shall require individual Special Use Permit approval.



Lot Requirements

Lot Area (See Table 7.1 for lot type & allocation guidelines)

Single family	3600 sf. min.
Townhouse	1500 sf min.
Multi family	per site plan
Non-residential	per site plan

Lot Width

Single family	36' min.
Town house	16' min.
Multifamily	per site plan
Non-residential	per site plan

Lot Depth

Townhouse	75' min.
Single Family	85' min.
Multifamily	per site plan
Non-residential	per site plan

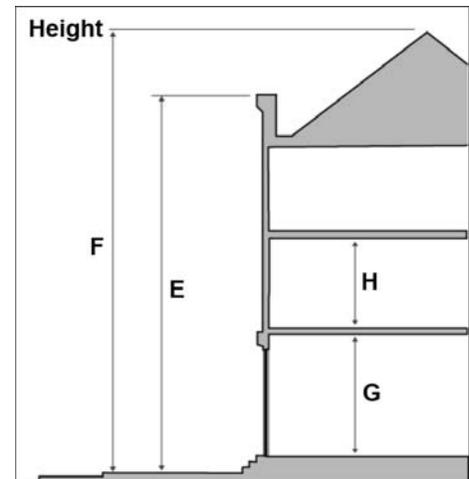
Lot Coverage	80% max. residential
	95% max. other

Block Length	600' max.
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Lot Access	Each lot must front a public street
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Building Height

E	Building Height to eave	40' max. (2-4 stories)
F	Building Height overall	50' max. (2-4 stories)
G	First floor ceiling height	12' min. retail
		10' min. other non-residential
		9' min. civic / live-work
		8' min. residential
H	Upper floor ceiling height	8' min. clear



Residential Encroachment

Front Porch (unenclosed) max. 4' into minimum residential front setback
Chimney max. 1' into minimum residential side setback
(applicable to single family, townhouse, and live-work units.)

Frontage Types

Porch

I Depth 6' min. – 12' max.

Stoop

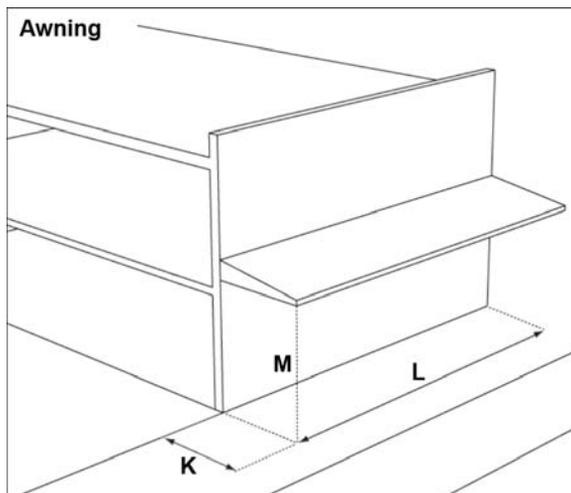
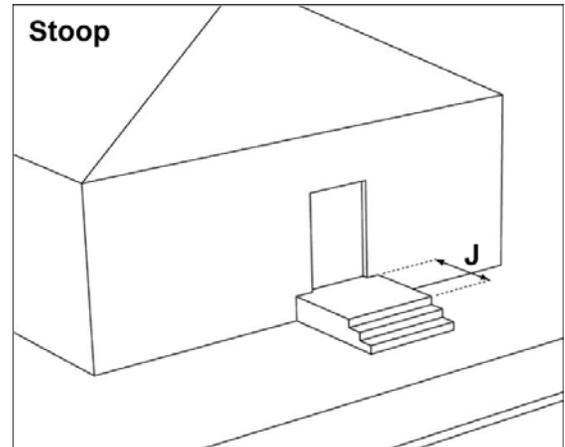
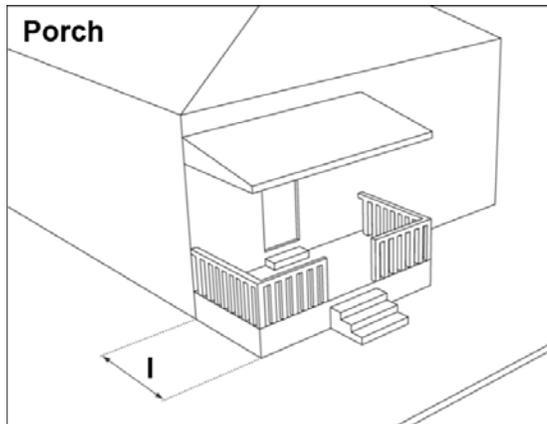
J Depth 5' min. – 12' max.

Awning

K Depth 12' max.

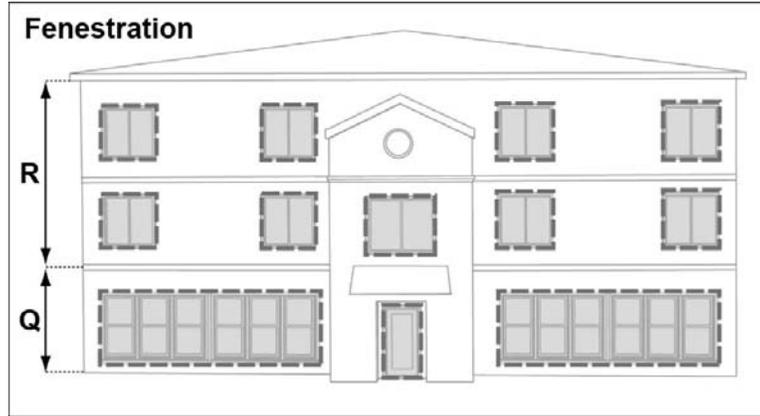
L Width 12' max.

M Height 8' min. clear



Fenestration

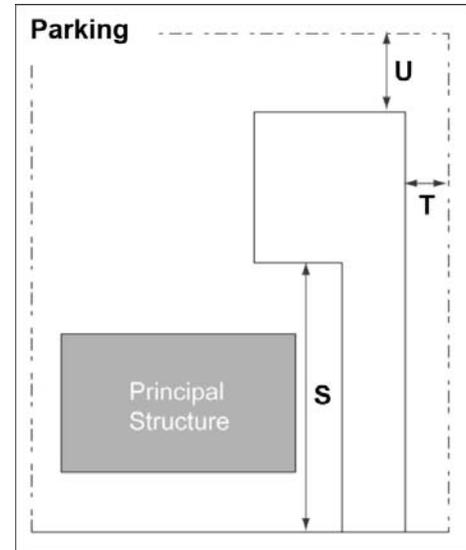
Q	Street Level	<u>40% min. retail</u> <u>20% min. other commercial</u> <u>30% min. residential</u>
R	Upper Floors	<u>15% min. – 40% max.</u>



Parking

S	Front Setback	<u>per site plan</u> <u>res.: 15' behind front façade</u>
T	Side Setback	<u>2' min.</u> <u>5' min. corner lots</u>
U	Rear Yard Setback	<u>3' min. from PL or</u> <u>alley easement.</u>

Parking must be visually screened from side and rear lot lines either by the principal structure, or by 3'-6' opaque fence and/or vegetative screen.



Accessory Structures

Setbacks	
Front	<u>30' min.</u>
Side	<u>2' min.</u> <u>5' min. corner lots</u>
Rear	<u>3' min.</u> <u>8' min adjacent to T3</u>
Height	
First floor ceiling	<u>8' min.</u> <u>8' min. (garage)</u>

Commercial and mixed-use buildings shall not have accessory structures

Streets

The Round Hill development will be served by a variety of street types, each designed to accommodate expected traffic volumes and compliment planned adjacent land uses. Individual Streetscape Section designs specify the location and size of travel lanes, on-street parking areas, landscaping zones, and sidewalks, and locate street trees, crosswalks, and other streetscape features.

The following Streetscape Sections apply, in whole or in part, to development in Round Hill's T5B zone:

- Major Village Street
- Major Village Street – Mixed Use
- Minor Village Street
- Minor Village Street – Mixed Use
- Radney Road
- Private Residential Alley

The applicable streetscape sections is presented in the adopted Street Map Diagram (refer to Exhibit 4.2 in Section 4 of this ordinance.)

Streetscape Standards

The streetscape standards herein below are provided to ensure the coherence of streets within the T5B-Center Transect zone. In addition to other sections of this ordinance, these standards serve to assist building owners and operators with the understanding of the relationship between the street and privately-owned lots. These standards are intended to establish an environment that encourages and facilitates pedestrian activity. The streetscape standards for the T5B-Center Transect zone are prescribed below, provided that the requirements of this subsection shall apply to a subdivision and/or development subject to the requirement for a site development plan as well as to lot development plans for individual detached and duplex dwellings. Variations from these standards may be approved by the Planning Commission upon submission of a statement of justification and the presentation of satisfactory alternatives by the applicant.

1. Street Trees:

- (a) A street tree plan shall be submitted with any public right of way infrastructure plan and plats.
- (b) Street trees shall be installed by the developer along both sides of all streets within the T5B-Center Transect zone.
- (c) Street trees shall be spaced at regular intervals of not fewer than 30 and not greater than 50 feet as measured parallel to the centerline of the street.

(d) Street trees shall be limbed up so as to not interfere with pedestrian or automobile travel. A minimum vertical clearance of 7 feet shall be provided above the sidewalk and 14 feet above the travel lanes of the street.

(e) When street trees are to be located in the sidewalk or other paved civic area, tree grates of a design, material, and location subject to final site plan approval shall be installed around street tree pit areas to prevent pedestrian damage to planting materials.

2. Sidewalks and Pedestrian Facilities:

(a) Sidewalks shall be installed by the developer along both sides of all public streets within the T5B-Center Transect zone. Where the T5B-Center Transect street section is contiguous with a T2-Open Space Transect, a system of internal pedestrian trails with the T2-Transect may be substituted for the required sidewalk on the T2 transect side of the street, subject to review by the Community Development Director and approval by the Planning Commission.

(b) Sidewalks shall not be fewer than 5 feet in width when located within a public right of way and shall otherwise be installed in accord with the approved street section. Sidewalks shall otherwise be designed and constructed to meet all applicable handicap access regulations.

(c) Sidewalks in excess of 8 feet in width may not be approved for dedication within the public right of way, at the discretion of the Town and VDOT upon review of the final site plan and subdivision plat. Where sidewalks (or similarly paved surfaces intended for common usage) in excess of 8' in width are intended to accomplish the streetscape and civic space objectives of the applicant, the additional width shall be incorporated into the lot. Where deemed necessary by the Planning Commission, a private access easement dedicated to public use may be required to accomplish the unified streetscape and civic space goals.

(d) Consistency of paving design shall be required within the development project.

3. Alleys: The private access easement for any alley shall be not fewer than 16 feet and not more than 24 feet in width. Alley easements shall be platted and recorded prior to the sale of any lot that would be served by the alley. The oversight and maintenance of alleys shall be the responsibility of a property owners' association that shall be established prior to the sale of the first lot within the section of development in which the alley is to be located.

4. Common Civic and Open Space within Transect Zone:

(a) A minimum of one tot lot or playground shall be developed on a separate lot reserved and maintained for community use within each transect zone. (*See "Parks, Open Space, and Recreation Areas" Map Exhibit 4.3 in Section 4*).

(b) At least five percent (5%) of the total land area included as part of an individual development project involving the construction of forty (40) or more townhouse or multifamily dwelling units shall be preserved as common open space within the transect.

This provision shall not apply to individual single-family, duplex, mixed-use, or live-work dwellings.

5. Private Property Lighting: Site lighting shall be located and shall be of a design and height so as to illuminate only the interior of the property. A plan for the outdoor lighting design for a lot or project within the T5B-Center Transect zone shall be included with the site plan or lot development plan.
6. Private Property Landscaping: Landscaping on individual lots and projects shall be compatible with the street tree planting plan on public streets. A plan for the private lot or project landscape design for a lot or project within the T5B-Center Transect zone shall be included with the site plan or lot development plan, and shall be compatible with all Town of Orange landscape ordinances.
7. Street Lighting:

(a) A street lighting plan shall be included with the submission of public improvements plans for all streets within the UNO-TND.

(b) Street lighting shall be installed by the developer concurrently with the development of the public right of way.

(c) Street lights shall be installed along both sides of all streets. The Lumec Candela CAND1-CN1 with 100-watt metal halide bulbs, or equivalent model in compliance with the Town of Orange Public Works Manual.

(d) Street lights shall be installed at least 12 feet and not more than 20 feet in height above grade and at regular intervals of not greater than 200 feet on center measured parallel to the centerline of the street.

(e) In order to minimize light pollution, light should be directed downward to the immediate area being illuminated and away from any living quarters.

(f) Floodlights or directional lights with maximum 100-watt metal halide bulbs may be used to illuminate alleys, parking garages and maintenance areas, but shall be shielded or aimed in such a way that they do not direct light onto other lots, the street, or out of the parcel.

(g) Lights shall not be placed on the ground such that a beam of light is directed upward to illuminate the building wall(s). This provision shall not apply to floodlighting used to illuminate flags.

(h) Site lighting shall be located and shall be of a design and height so as to illuminate only the interior of the property.



Lumec Candela CAND1-CN1

- (i) No flashing, traveling, animated, or intermittent lighting shall be visible from the exterior of any building whether such lighting is of temporary or long-term duration. This provision shall not apply to seasonal (i.e. holiday) lighting and/or displays.

Street Furniture

Streetscape furniture standards are provided to ensure the coherence of streets within the T5B-Center Transect. The standards serve to assist commercial, mixed-use, and high density residential building owners and operators with understanding the relationship between the street and their privately-owned lots. The street and building façade receives equal attention to the rest of the building. The desired aesthetic along the public right of way, civic spaces and the private area in front of commercial, mixed-use, and high density residential buildings is defined by streetscape elements, such as brick pavers, benches, lighting and waste-bins, street trees and public art. These standards, in concert with the Streetscape Standards, establish an environment that encourages and facilitates pedestrian activity.

1. Benches with backs and arm rests shall be provided at regular intervals not exceeding 250 feet along the street frontage, on average, measured parallel to the street beginning at the intersection of any two streets. The Victor Stanley Steelsites Model No. R-B 28 or equivalent model approved with the final site plan and plat shall be used within the T5B-Center Transect.
2. At a minimum, one (1) waste bin shall be provided at each block corner with the site improvements for (a) non-residential, mixed-use, or multifamily building use or (b) a parks, recreation or open space lot, with placement subject to final design determinations. The Victor Stanley Steelsites Model No. NSDC-36 or equivalent model approved with the final site plan and subdivision plat shall be used within the T4 Transect.
3. Bicycle racks having a minimum two-bike capacity shall be installed along both sides of the street (at no more than 500-foot intervals, measured parallel to the street beginning at the intersection of any two streets) with the site improvements for (a) non-residential, mixed-use or multifamily building use, or (b) a parks, recreation or open space lot. The standard bike rack is an inverted “U” in galvanized steel with a baked-on black paint finish or as may be otherwise approved by the Planning Commission.
4. The placement, model and type of all street furniture and fixtures shall be of a design, placement, and construction specification subject to final site plan approval.



Victor Stanley Steelsites Model No. NSDC-36



Victor Stanley Steelsites Model No. R-B 28

Additional Transect Development Standards

1. Minimum 30 feet frontage for single family residential detached dwellings, where permitted, provided that cul-de-sac lots and “flag” (or “stem”) lots with less public street frontage may be approved on an individual basis by the Community Development Director. Public frontage requirements are not regulated for townhouse and duplex, but lot frontage geometry is subject to site plan review and approval. The total number of flag lots within the T5B Transect shall not exceed five percent (5%) of the total number of approved lots within the transect. A flag lot shall be limited to providing access to and accommodating a single dwelling unit. The entirety of the frontage and drive aisle portion of a flag lot shall be under individual ownership with its geometric configuration integral to that of the platted lot and building that it serves.
2. A residential lot development plan shall be required with zoning permit applications for single family detached and duplex dwelling units. The zoning permit shall be issued upon approval of the lot development plan by the Community Development Director. Townhouses, multifamily residences and other uses shall be subject to site plan and subdivision plat approval. See “SFD/Duplex Lot Development Plan” sub-section herein below.
3. Lot access: All single family detached residential lots which contain a residential dwelling or other permitted main building shall abut a public street.
4. Corner Lots: Of the two (2) sides of a corner lot, the front lot line shall be deemed to be the shorter of the two sides adjoining the street.
5. Street Terminus and Cul-de-sacs: Any temporary, dead-end street must terminate in a hammerhead or other alternative turnaround design located within a dedicated public right of way, provided, however, that a permanent cul-de-sac may be permitted.

- 6. Roof Pitch: Not regulated.
- 7. Accessory Structures: The footprint of an accessory structure shall not exceed 50 percent (50%) of the footprint of the principal dwelling up to a maximum area of 1,000 square feet. The height of the accessory structure shall not exceed the height of the principal dwelling. All accessory structures must be set back from the front lot line far enough so that at least 50 percent (50%) of the mass or depth of the principal structure is closer to the front lot line than is any portion of the accessory structure. Accessory structures shall not be allowed for non-residential uses.
- 8. Calculation of Minimum and Maximum Residential Dwelling Count:

- MXPRD:** Maximum permitted residential dwellings (*in dwelling units*)
- MXPD:** Maximum permitted density in transect (*in dwelling units per acre*)
- MNPRD:** Minimum permitted residential dwellings (*in dwelling units*)
- MNPD:** Minimum permitted density in transect (*in dwelling units per acre*)
- TGLA:** Transect gross land area (*in acres*)
- TANR:** Transect area allocated to non-residential and mixed-use (*in acres*)

The maximum number of residential dwelling units permitted within the T5B Transect is calculated as follows:

$$\begin{aligned} & \textbf{Maximum Permitted Residential Dwellings} = \\ & \textbf{(Maximum Permitted Density in T5B Transect, in dwelling units per acre) X} \\ & \textbf{[(Total T5B Transect Gross Land Area, in acres) -} \\ & \textbf{(T5B Transect Area Allocated to Non-Residential and Mixed-Use, in acres)]} \end{aligned}$$

Abbreviated formula:

$$\textbf{MXPRD} = \textbf{MXPD X (TGLA - TANR)}$$

The minimum number of residential dwelling units permitted within the T5B Transect is calculated as follows:

$$\begin{aligned} & \textbf{Minimum Permitted Residential Dwellings} = \\ & \textbf{(Minimum Permitted Density in T5B Transect, in dwelling units per acre) X} \\ & \textbf{[(Total T5B Transect Gross Land Area, in acres) -} \\ & \textbf{(T5B Transect Area Allocated to Non-Residential and Mixed-Use, in acres)]} \end{aligned}$$

Abbreviated formula:

$$\textbf{MNPRD} = \textbf{MNPD X (TGLA - TANR)}$$

9. Transfer of Maximum Residential Dwelling Units Between Transects:

Up to five percent (5%) of the approved dwelling units within another transect may be transferred to the subject transect upon review and approval by the Planning Commission and, further, provided that (a) the maximum allowable number of units in the transferring transect (calculated by the formula in 8. hereinabove) shall be reduced by the number of units transferred, and (b) the increase in the total number of units in the receiving transect shall not exceed five percent (5%) of the number of units originally approved for the transect.

In transects where transference of dwelling units has been approved by the Town, the each lot development plan, site plan, and subdivision plat submitted to the Town shall include the following documentation:

- (a) a calculation of the cumulative dwelling units previously approved within the T5B Transect,
- (b) the cumulative number of dwelling units that have been applied to the individual project from the identified transferring transect, and
- (c) the cumulative number of of dwelling units that have been reduced in the transferring transect and the maximum number of permitted dwellings remaining in that transect.

Once the maximum allowable residential dwelling units within the transect have been approved for construction by the Town, no additional dwelling units may be approved to be constructed within the transect, provided that any subsequent modification to the maximum number of dwelling units shall be subject to a formal amendment to the Round Hill / UNO-TND Code of Development and the transect zoning regulations herein.

10. Calculation of Minimum and Maximum Building Floor Area for Non-Residential and Mixed-Used Buildings:

MXPFA: Maximum floor area of non-residential and mixed use buildings
(in square feet gross floor area, *sfgfa*)

MXPFAR: Maximum permitted floor area ratio, FAR (a unit-less ratio)

MNPFA: Minimum floor area of non-residential and mixed use buildings
(in square feet gross floor area, *sfgfa*)

MNPFAR: Minimum permitted floor area ratio, FAR (a unit-less ratio)

TGLA: Transect gross land area (in acres)

TAR: Transect area allocated to residential areas (in acres)

The maximum gross floor area for non-residential and mixed-use buildings permitted within the T5B Transect is calculated as follows:

$$\begin{aligned} & \textbf{Maximum Gross Floor Area for Non-Residential and Mixed-Use Buildings} = \\ & \textbf{(Maximum Permitted Floor Area Ratio (FAR) in T5B Transect) X} \\ & \textbf{[(Total T5B Transect Gross Land Area, in square feet) -} \\ & \textbf{(T5B Transect Area Allocated to Residential Areas, in square feet)]} \end{aligned}$$

Abbreviated formula:

$$\text{MXPFA} = \text{MXPFAR} \times (\text{TGLA} - \text{TAR})$$

The minimum gross floor area for non-residential and mixed-use buildings permitted within the T5B Transect is calculated as follows:

$$\begin{aligned} & \textbf{Minimum Gross Floor Area for Non-Residential and Mixed-Use Buildings} = \\ & \textbf{(Minimum Permitted Floor Area Ratio (FAR) in T5B Transect) X} \\ & \textbf{[(Total T5B Transect Gross Land Area, in square feet) -} \\ & \textbf{(T5B Transect Area Allocated to Residential Areas, in square feet)]} \end{aligned}$$

Abbreviated formula:

$$\text{MNPFA} = \text{MNPFAR} \times (\text{TGLA} - \text{TAR})$$

Once the maximum allowable gross floor area for non-residential and mixed-use within the transect has been approved for construction by the Town, no non-residential and mixed-use floor area may be approved to be constructed within the transect, provided that any subsequent modification to the maximum gross floor area shall be subject to a formal amendment to the Round Hill / UNO-TND Code of Development and the transect zoning regulations herein.

See *Appendix A* for an illustrative example of the procedure to calculate minimum and maximum permitted yield and mix of residential and non-residential/mixed uses in the T5A Transect Zone.

11. Residential units are permitted on the upper stories of mixed-use buildings above the ground floor non-residential footprint. The number and space allocated to dwelling units that are located in mixed-use buildings are regulated by the equation for minimum and maximum FAR (*see 10. above.*) and shall not be included in the count of maximum and minimum number of permitted residential dwelling units (*see 8. above.*)
12. Adjustment to Transect Area and Location: The approved location and acreage within the T5B Transect is as indicated on the adopted Uptown North Orange TND Transect Map Diagram (Exhibit 4.1 in Section 4). Minor adjustments to the transect boundary and acreage may be approved by the Community Development Director subject to demonstration of need by the applicant based on final site plan, civil, engineering or geophysical determinations. Any variation to the size of the T5B Transect that increases or decreases the gross transect area by $\geq 10\%$ shall be subject to review and approval by the Planning Commission. Any change in transect area and acreage shall be documented by the applicant on a revised Transect Map Diagram. If approved, the revised Transect Map Diagram shall govern.

Recreational Facility Standards

Individual multi-family and townhouse projects of forty (40) dwelling units or more shall provide improved recreational space for the use of residents, including facilities such as playgrounds, tot lots, civic space, tennis courts, swimming pools and clubhouses. A property owners' (or homeowners') association agreement shall be responsible for the ownership and maintenance of common property within the project.

Applicants for townhouse and multifamily projects with fewer than forty dwelling units shall demonstrate that the recreation and open space improvements in the planned and existing T2 Open Space transects are properly located and adequate to provide for the needs of project residents. If not adequately demonstrated, the Planning Commission, upon recommendation from the Community Development Director, may require recreational improvements within the proposed project.

All recreational and open space improvements shall meet the following minimum standards and supplementary requirements:

1. The size, type, and number of facilities provided shall be commensurate with the anticipated needs of the project's residents. Recreational improvements shall be coordinated with other open space and recreational improvements in the T2 Open Space Transect. Improvements to be located within the T2 Open Space Transect shall be taken into consideration in determining the relationship between supply and demand for said facilities. In determining the extent of necessary facilities, the applicant shall meet with the Community Development Director to review a proposed recreation master plan. The applicant shall provide construction drawings and specifications for proposed facilities at the time of site plan and subdivision plat submittal.
2. In determining the extent of necessary facilities, the applicant shall meet with the Community Development Director prior to submission of final site plan in order to:
 - (a) coordinate improvements with the overall plan of open space and recreation improvements planned or existing in the T2 Open Space Transect Zone,
 - (b) coordinate improvements with the tot lot or recreation area planned or located within the transect in which the project is located, and
 - (c) finalize the specific recreational improvements to be located within the proposed project. The applicant shall provide construction drawings and specifications for proposed facilities at the time of site plan and subdivision plat submittal.
3. Slopes should not exceed ten percent (10%) and the area for the placement of recreational facilities.
4. The size and shape of each recreation area shall be adequate for the intended use.
5. Groundcover shall be appropriate for the use and shall consist of turf grass or contained ground cover such as pine bark mulch, shredded tires, or pea gravel.
6. Location, grading, and facility placement shall be compatible with adjoining uses, convenient to users, and suitable for parental supervision.
7. All recreational equipment and facilities shall be designed for regular and extended use. Recreational equipment and facilities shall be maintained in a safe and attractive condition and replaced as necessary. Maintenance shall be the responsibility of the property owners' or homeowners' association.
8. All required recreational facilities shall be completed or bonded for completion by the time of issuance of a Certificate of Occupancy for the units requiring the facilities.
9. A property owners' (or homeowners') association agreement and articles of incorporation shall be established and recorded with each project. Such agreements shall address the ownership and maintenance responsibilities of recreational land and improvements.

Parking

1. The TND District encourages creative parking strategies that are aimed at balancing gross parking requirements for the project with opportunities for shared parking, on-street parking, phasing of parking, and coordination with environmentally responsive, low impact development techniques. This objective may be pursued at the time application for individual site plan and subdivision plat approval (*see 9. and 10. herein below.*)
2. On-street parking is permitted in accord with individual street sections included in this Code of Development and as may be subsequently approved with final site plan and subdivision plats for individual projects.
3. In addition to on-street parking, off-street parking shall be provided on the individual lot subject to the zoning permit application. Parking on private property may be accessed via a driveway or drive aisle directly from the public street on which the lot fronts or via an approved alley located within the private access easement. Off-street parking access from a public street is subject to approval of a final site plan and subdivision plat.
4. Garages serving single family detached residential dwellings accessed from a public street frontage must be set back a minimum of 15' from the front building line as established for the residential unit by the SFD/Duplex Lot Development Plan.
5. Residences located in the upper levels of mixed-use buildings shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces. Not more than seventy-five percent (75%) of the required residential off-street spaces may be located within a garage or parking structure.
6. Townhouse and single family detached residences shall provide two (2) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage or parking structure.
7. Freestanding multi-family residences shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage or parking structure.
8. Commercial retail, office, and other non-residential uses within the transect shall conform to the parking requirements of the Town of Orange. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. hereinbelow.*) The required commercial and other non-residential off-street spaces may be located within a parking structure, subject to final site plan and plat approval.
9. Applicants are encouraged to evaluate the benefits of shared parking and other parking reduction opportunities, where appropriate. The procedure for this is through the

submission of a parking impact study. If the applicant elects to establish parking requirements based on a parking impact study, the study shall be subject to review and approval by the Planning Commission. The applicant shall prepare the parking impact study employing contemporary research and parking projection techniques, including studies prepared by the Urban Land Institute.

The parking impact study for a given project shall address: (a) planned and existing on-street parking supply, (b) shared-parking opportunities with other planned and existing residential and non-residential development projects, (c) mixed-use parking relationships, (d) location of project within the transect, (e) development phasing, (f) coordination with runoff reduction techniques, and (g) demonstration of total parking adequacy to meet the marketplace demands of the transect and project. The project-specific scope of the parking impact study shall be established at a meeting with the Community Development Director. The parking impact study shall be prepared by a qualified professional.

10. The parking impact study shall be submitted to the Community Development Director for review and approval at a minimum of thirty (30) days prior to submittal of the final site plan or subdivision plat for the project. Upon review, the Community Development Director shall provide written comment to the applicant which shall be reflected in the final site plan.

Upon review of the parking impact study and the subsequent the review of the final site plan or subdivision plat, the Planning Commission may act to approve, deny, or amend with appropriate conditions the applicant's request for a reduction in the number of parking spaces or other aspects of parking requirements that would otherwise be required by the application of Town parking standards.

SFD/Duplex Residential Lot Development Plan

1. A lot development plan is a requirement of a zoning permit application for a duplex or single family detached residential unit(s). The lot development plan, prepared in accord with the requirements of the Section 6 of this ordinance, shall be submitted to the Town with the zoning permit application. Multiple units may be included on a lot development plan.
2. The lot development plan shall be (a) submitted with the zoning permit application, (c) demonstrate compatibility with the requirements herein above, (c) provide a cumulative total of the number of dwelling units previously approved for the transect, (d) exhibit consistency with the approved final site plan, transportation and utility plans, and subdivision plat for the block or transect infrastructure in which the building is proposed, and (e) incorporate the documentation and calculations required in *Additional Transect Development Standards, Paragraph. 9.*

Utility Requirements

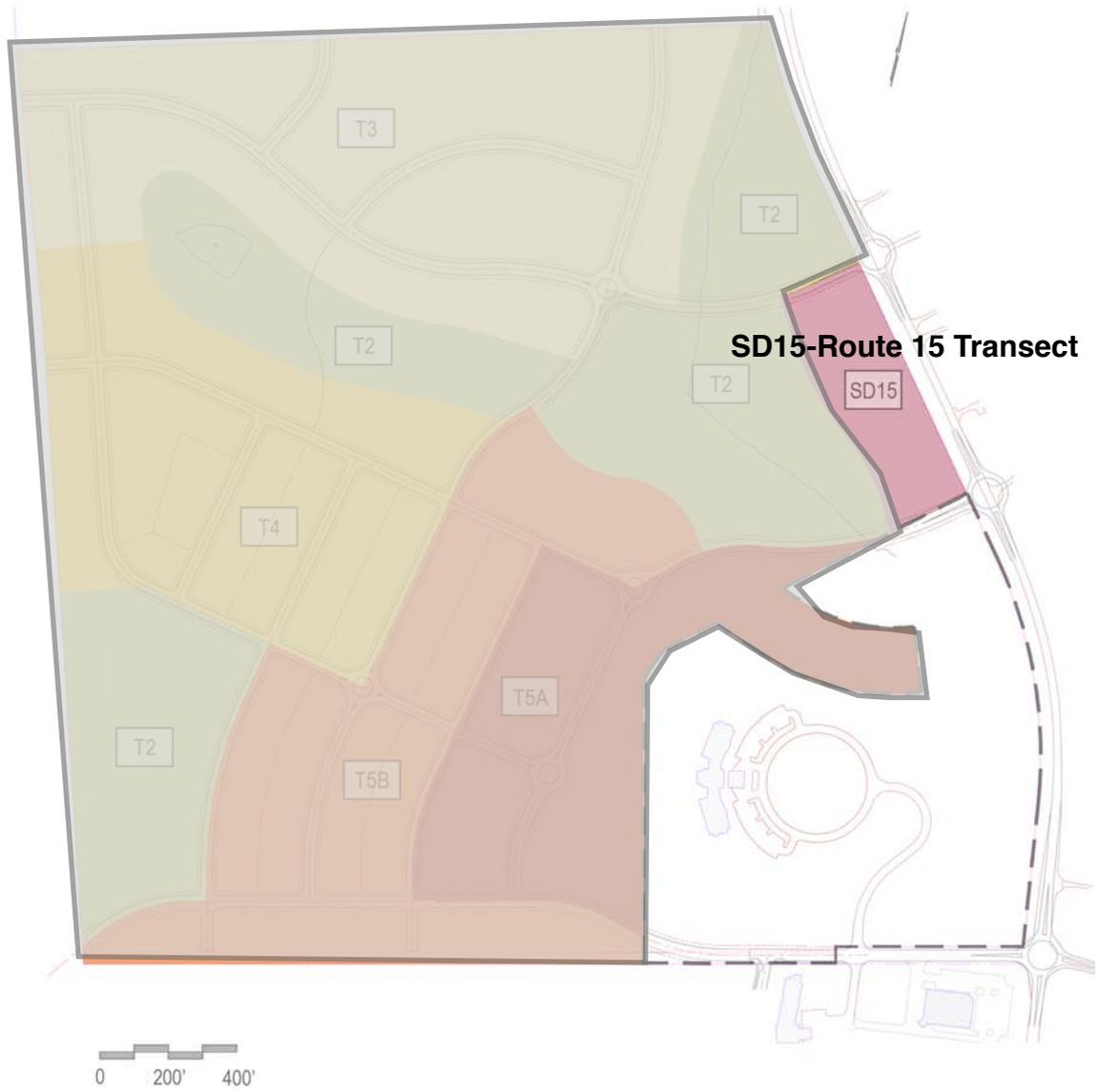
1. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public water system.
2. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public sewer system.
3. All utilities that serve a structure shall be underground. Temporary construction shall be exempted from this requirement.

Special Administrative Standards: Modifications

1. The Town recognizes a process by which to obtain “major modifications” and “minor modifications” to the TND District transect zone standards contained herein. Modifications to the transect zone standards that are deemed “major modifications” by the Community Development Director, in consultation with the Town Attorney, shall require the applicant to request a formal amendment to the approved zoning of the TND District. An amendment to the zoning shall be processed by the Town in the same manner as an original zoning map amendment.
2. Modifications to the approved transect standards that are deemed “minor modifications” by the Community Development Director, in consultation with the Town Attorney, that are not otherwise delegated hereinabove to the Community Development Director may be approved by the Planning Commission upon submission of a statement of justification for the modification and the presentation of satisfactory alternatives by the applicant.
3. The Planning Commission shall consider the recommendation of the Community Development Director and other qualified sources in consideration of any minor modification from the approved transect standards hereinabove.
4. Requests for minor modifications from an approved transect standard shall be heard at a regularly scheduled meeting of the Planning Commission.

SD15 Transect Zone

4.40-acres



Round Hill/UNO-TND District

The Town of Orange

SD15 Special District Transect Zone

Intent

The SD15 Transect zone is a special transect within the UNO/Round Hill TND that, due to its adjacency to Route 15, has a physical form that is intended to be more automobile-oriented than other internal Round Hill transect zones. Due to the transect’s highway visibility and high traffic volumes coupled with the Town’s retail development objectives for the Route 15 corridor, the SD15 Transect Zone is a highly desirable location for the type of highway-oriented destination retail that can serve as a catalyst for the Uptown North Orange planning area. High quality, “four-sided” architecture is an expectation of development due to the transect’s visual orientation to both the project and the Route 15 corridor. Large-footprint retail commercial establishments are envisioned to support and sustain the smaller-footprint, pedestrian-oriented “town center” establishments that are planned for the T5 Transects. Such large footprint, commercial establishments may be supported by townhouse or multifamily, subject to special use permit approval. Single family residential dwellings are not permitted in this district.

Area and Density

The approved land area and the general location of the SD15 Transect Zone is depicted on the adopted Uptown North Orange TND Transect Map Diagram (Exhibit 4.1 in Section 4.) The minimum and maximum non-residential/mixed-use density, expressed in floor area ratio (FAR), is as follows:

Minimum Non-Residential/Mixed-Use Density: 0.20 FAR
Maximum Non-Residential/Mixed-Use Density: 1.00 FAR

The mix and yield for residential development in the transect is subject to special use permit approval, and, further, subject to the following minimum and maximum residential density, expressed in dwelling units per acre:

Minimum Residential Density: 12.0 dwelling units per acre.
Maximum Residential Density: 24.0 dwelling units per acre.

(See “Additional Transect Development Standards” section for the method of calculation of the minimum and maximum permitted number of residential dwelling units and the minimum and maximum gross floor area of non-residential and mixed-used development within the SD15 Transect Zone.)

Permitted Uses

The uses permitted by-right and permitted with a special use permit within the SD15 Transect Zone shall be in accordance with the Table of By-Right and Special Permitted Uses (Section 3.)

Mix of Uses

A mix of residential and non-residential/mixed-uses shall be developed in the SD15 Transect Zone. The following represents the permitted range in the allowable mix of uses, expressed as a percentage of the total SD15 Transect Zone land area:

<i>Minimum Residential Transect Area:</i>	<i>0%</i>
<i>Maximum Residential Transect Area:</i>	<i>50%</i>
<i>Minimum Non-Residential/Mixed-Use Transect Area:</i>	<i>50%</i>
<i>Maximum Non-Residential/Mixed-Use Transect Area:</i>	<i>100%</i>

See Appendix A for an illustrative example for the calculation of minimum and maximum permitted mix of residential and non-residential/mixed uses in the SD15 Transect Zone.

Development Standards

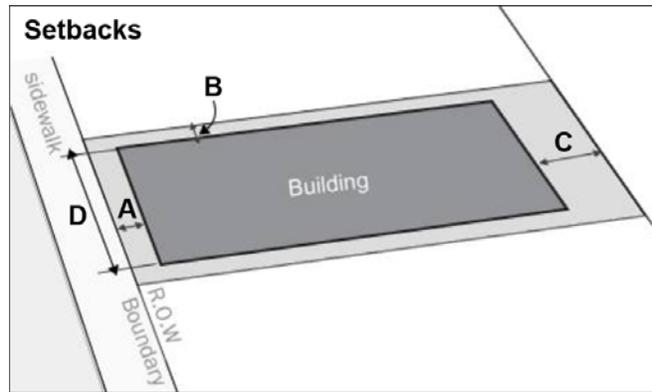
The general development standards in the SD15 Transect for all lots and buildings are regulated by the following tables and illustrations.

Building Requirements

Setbacks

A	Front from Route 15 ROW	15' min.
	Front from internal street PL	15' min.
B	Side setback from ROW or PL	15' min.
C	Rear	not regulated
D	Facade buildout along frontage	not regulated

Maximum building footprint per non-residential establishment: 60,000 square feet gross floor area on the first floor. Establishments up to 80,000 square feet gross floor area on the first floor may be allowed by individual Special Use Permit approval.



Lot Requirements

Lot Area (See Table 7.1 for lot type & allocation guidelines)

Townhouse	1500 sf min.
Multi family	per site plan
Non-residential	per site plan

Lot Width

Town house	16' min.
Multifamily	per site plan
Non-residential	per site plan

Lot Depth

Townhouse	75'
Multifamily	per site plan
Non-residential	per site plan

Lot Width per site plan approval

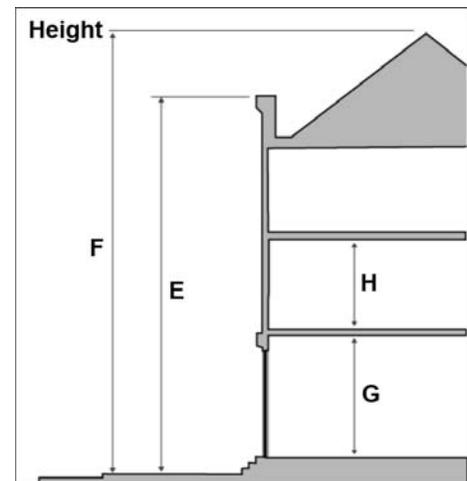
Lot Coverage 90% max.

Block Length per site plan approval

Lot Access Each lot must front a public street

Building Height

E	Commercial Building Height to Eave	30' max.
F	Commercial Building Height overall	40' max.
G	Mixed-use / Multifamily Building Height to Eave	40' max.
H	Mixed-use / Multifamily Building Height overall	50' max.



Accessory Structures

Setbacks

Front	15' min. behind front facade
Side	2' min.
Rear	8' min adjacent to T3 5' min. from PL or alley easement.
Height	No taller than primary build.

Commercial and mixed-use buildings shall not have accessory structures

Streets

The Round Hill TND development will be served by a variety of street types, each designed to accommodate expected traffic volumes and compliment planned adjacent land uses. Individual Streetscape Section designs specify the location and size of travel lanes, on-street parking areas, landscaping zones, and sidewalks, and locate street trees, crosswalks, and other streetscape features.

The following Streetscape Sections apply, in whole or in part, to development in Round Hill's SD15 zone:

- Major Village Street
- Major Village Street – Mixed Use

The applicable streetscape sections are presented in the adopted *Thoroughfare Map Diagram* (See Map Exhibit 4.2 in Section 4).

Streetscape Standards

The streetscape standards herein below are provided to ensure the coherence of streets and interior private travelways within the SD15 Zone transect zone. In addition to other sections of this ordinance, these standards serve to assist the developer, builders, owners, and operators with the understanding of the relationship between the street and privately-owned lots. These standards are intended to establish an environment that promotes safety and coherence between vehicular and pedestrian movements. The streetscape standards for the SD15 Zone transect zone are prescribed below and shall be implemented to the maximum extent practicable.

1. Street Trees:

(a) A Route 15 corridor master plan shall be submitted with the first site plan or subdivision plat for development within the SD15 transect. The corridor master plan shall address the preliminary design, type, and location of right of way improvements and contiguous lot improvements, including street trees, supplemental landscaping, outdoor lighting and street lighting, sidewalks, signage, storm drainage, and other essential infrastructure. Subsequent lot development in the transect shall confirm to the maximum

extent practicable with the corridor landscape master plan and shall coordinate improvements within the public right of way and private lot development.

(b) A street tree plan shall be submitted with any site plan, public right of way infrastructure plan, and subdivision or easement plats. Street trees shall be installed by the developer along both sides of all internal streets and travelways within the SD15 Zone transect zone.

(c) Street trees shall be spaced at regular intervals of not fewer than 30 and not greater than 60 feet as measured parallel to the centerline of the street.

(d) Street trees shall be limbed up so as to not interfere with pedestrian or automobile travel. A minimum vertical clearance of 7 feet shall be provided above the sidewalk and 14 feet above the travel lanes of the street.

(e) When street trees are to be located in the sidewalk or other paved civic area, tree grates of a design, material, and location subject to final site plan approval shall be installed around street tree pit areas to prevent pedestrian damage to planting materials.

2. Sidewalks and Pedestrian Facilities:

(a) Sidewalks shall be installed by the developer along both sides of all public streets within the SD15 Zone transect zone. The requirement for sidewalks on interior travelways and parking areas that are not addressed by the approved UNO/Round Hill street sections shall be subject to site plan review and approval.

(b) Sidewalks shall not be fewer than 5 feet in width when located within a public right of way and shall otherwise be installed in accord with the approved street section. Sidewalks shall otherwise be designed and constructed to meet all applicable handicap access regulations.

(c) Sidewalks in excess of 8 feet in width may be approved for dedication within the public right of way at the discretion of the Town and VDOT upon review of the final site plan and subdivision plat. Where sidewalks (or similarly paved surfaces intended for common usage) in excess of 8' in width are intended to accomplish the streetscape and civic space objectives of the applicant, the additional width shall be incorporated into the lot. Where deemed necessary by the Planning Commission, a private access easement dedicated to public use may be required to accomplish the unified streetscape and civic space goals.

(d) consistency of paving design shall be required within the development project.

3. Alleys: The private access easement for any alley in the SD 15 transect shall be not fewer than 16 feet and not more than 30 feet in width. Alley easements shall be platted and recorded prior to the sale of any lot that would be served by the alley. The oversight and maintenance of alleys shall be the responsibility of a property owners' association that shall be established prior to the sale of the first lot within the section of development in which the alley is to be located.

4. Common Civic and Open Space within Transect Zone:

(a) A minimum of one tot lot or playground shall be developed on a separate lot reserved and maintained for community use within each transect zone. (See “Parks, Open Space, and Recreation Areas” Exhibit 4.3 in Section 4).

(b) At least five percent (5%) of the total land area included as part of an individual development project in the SDF15 Transect involving the construction of twenty-five (25) or more townhouse or multifamily dwelling units shall be preserved as common open space within the transect. This provision shall not apply to individual single-family, duplex, mixed-use, or live-work dwellings.

5. Private Property Lighting: Site lighting shall be located and shall be of a design and height so as to illuminate only the interior of the property. A plan for the outdoor lighting design for a lot or project within the SD15 Zone transect zone shall be included with the site plan or lot development plan.

6. Private Property Landscaping: Landscaping on individual lots and projects shall be compatible with the street tree planting plan on both public and private streets. A plan for the private lot or project landscape design for a lot or project within the SD15 Zone transect zone shall be included with the site plan or lot development plan, and shall be compatible with all Town of Orange landscape ordinances.

7. Street Lighting:

(a) A street lighting plan shall be included with the submission of a site plan or public improvements plans for all streets within the UNO TND.

(b) Street and parking lot lighting shall be installed by the developer of a lot within the SD15 transect concurrently with the development of the public right of way, private street, and parking areas.

(c) Site lighting shall be located and shall be of a design and height so as to illuminate only the interior of the property.

(d) Street lights shall be dark sky compatible. Lighting shall be designed and installed to be fully shielded (i.e. full cutoff) and shall have a maximum lamp wattage of 300 watts high-intensity discharge (HID) or lumen equivalent, or as otherwise approved during the site plan review process. In mixed-use and residential projects, light should be shielded such that the lamp itself or the lamp image is not directly visible outside the perimeter of the property.

(e) Street light luminaire fixtures and post standards shall be compatible with the Town of Orange requirements or as may otherwise be approved by the Planning Commission.

Additional Transect Development Standards

1. Minimum 30 feet lot frontage, provided that cul-de-sac lots and “flag” lots with less public street frontage or accessed by a private access easement may be approved on an individual basis by the Planning Commission.
2. Street Terminus and Cul-de-sacs: Any temporary, dead-end public or private street must terminate in a hammerhead or other alternative turnaround design located within a dedicated public right of way, provided, however, that a permanent cul-de-sac may be permitted.
3. Accessory Structures: The footprint of an accessory structure shall not exceed 50 percent (50%) of the footprint of the principal building up to a maximum area of 2000 square feet.
5. Calculation of Minimum and Maximum Residential Dwelling Count:

MXPRD: Maximum permitted residential dwellings (*in dwelling units*)

MXPD: Maximum permitted density in transect (*in dwelling units per acre*)

MNPRD: Minimum permitted residential dwellings (*in dwelling units*)

MNPD: Minimum permitted density in transect (*in dwelling units per acre*)

TGLA: Transect gross land area (*in acres*)

TANR: Transect area allocated to non-residential and mixed-use (*in acres*)

Freestanding townhouse and multifamily dwellings may be permitted in the SD15 Transect subject to a special permit approval. The maximum number of residential dwelling units (townhouse, multifamily) permitted within the SD15 Transect is calculated as follows:

$$\begin{aligned} & \text{Maximum Permitted Residential Dwellings} = \\ & (\text{Maximum Permitted Density in SD15 Transect, } in \text{ dwelling units per acre}) \times \\ & [(\text{Total SD15 Transect Gross Land Area, } in \text{ acres}) - \\ & (\text{SD15 Transect Area Allocated to Non-Residential and Mixed-Use Areas } in \text{ acres})] \end{aligned}$$

Abbreviated formula:

$$\text{MXPRD} = \text{MXPD} \times (\text{TGLA} - \text{TANR})$$

The minimum number of residential dwelling units (townhouse, multifamily) permitted within the SD15 Transect is calculated as follows:

$$\begin{aligned} & \textbf{Minimum Permitted Residential Dwellings} = \\ & \textbf{(Minimum Permitted Density in SD15 Transect, in dwelling units per acre) X} \\ & \textbf{[(Total SD15 Transect Gross Land Area, in acres) -} \\ & \textbf{(SD15 Transect Area Allocated to Non-Residential and Mixed-Use Areas, in acres)]} \end{aligned}$$

Abbreviated formula:

$$\textbf{MNPRD} = \textbf{MNPD X (TGLA - TANR)}$$

6. Transfer of Maximum Residential Dwelling Units Between Transects:

Up to five percent (5%) of the approved dwelling units within another transect may be transferred to the subject transect upon review and approval by the Planning Commission and, further, provided that (a) the maximum allowable number of units in the transferring transect (calculated by the formula in 8. hereinabove) shall be reduced by the number of units transferred, and (b) the increase in the total number of units in the receiving transect shall not exceed five percent (5%) of the number of units originally approved for the transect.

In transects where transference of dwelling units has been approved by the Town, the each lot development plan, site plan, and subdivision plat submitted to the Town shall include the following documentation:

- (a) a calculation of the cumulative dwelling units previously approved within the SD15 Transect,
- (b) the cumulative number of dwelling units that have been applied to the individual project from the identified transferring transect, and
- (c) the cumulative number of of dwelling units that have been reduced in the transferring transect and the maximum number of permitted dwellings remaining in that transect.

Once the maximum allowable residential dwelling units within the transect have been approved for construction by the Town, no additional dwelling units may be approved to be constructed within the transect, provided that any subsequent modification to the maximum number of dwelling units shall be subject to a formal amendment to the Round Hill / UNO-TND Code of Development and the transect zoning regulations herein.

See Appendix A for an illustrative example for the procedure to calculate minimum and maximum permitted yield and mix of residential uses in the SD15 Transect Zone.

7. Calculation of Minimum and Maximum Building Floor Area (FAR) for Non-Residential and Mixed-Used Buildings:

MXPFA: Maximum floor area of non-residential and mixed use buildings
(in square feet gross floor area, sfgfa)

MXPFAR: Maximum permitted floor area ratio, FAR (a unit-less ratio)

MNPFA: Minimum floor area of non-residential and mixed use buildings
(in square feet gross floor area, sfgfa)

MNPFAR: Minimum permitted floor area ratio, FAR (a unit-less ratio)

TGLA: Transect gross land area (area in square feet)

TAR: Transect area allocated to residential areas (area in square feet)

The maximum gross floor area for non-residential and mixed-use buildings permitted within the SD15 Transect is calculated as follows:

$$\begin{aligned} &\textbf{Maximum Gross Floor Area for Non-Residential and Mixed-Use Buildings} = \\ &\textbf{(Maximum Permitted Floor Area Ratio (FAR) in SD15 Transect) X} \\ &\textbf{[(Total SD15 Transect Gross Land Area, in square feet) -} \\ &\textbf{(SD15 Transect Area Allocated to Residential Areas, in square feet)]} \end{aligned}$$

The minimum gross floor area for non-residential and mixed-use buildings permitted within the SD15 Transect is calculated as follows:

Abbreviated formula:

$$\textbf{MXPFA} = \textbf{MXPFAR X (TGLA - TAR)}$$

$$\begin{aligned} &\textbf{Minimum Gross Floor Area for Non-Residential and Mixed-Use Buildings} = \\ &\textbf{(Minimum Permitted Floor Area Ratio (FAR) in SD15 Transect) X} \\ &\textbf{[(Total SD15 Transect Gross Land Area, in square feet) -} \\ &\textbf{(SD15 Transect Area Allocated to Residential Areas, in square feet)]} \end{aligned}$$

Abbreviated formula:

$$\textbf{MNPFA} = \textbf{MNPFAR X (TGLA - TAR)}$$

Once the maximum allowable gross floor area for non-residential and mixed-use within the transect has been approved for construction by the Town, no non-residential and mixed-use floor area may be approved to be constructed within the transect, provided that any subsequent modification to the maximum gross floor area shall be subject to a formal amendment to the Round Hill / UNO-TND Code of Development and the transect zoning regulations herein.

See *Appendix A* for an illustrative example for the procedure to calculate minimum and maximum permitted yield and mix of non-residential/mixed uses in the SD15 Transect Zone.

8. Residential units are permitted by-right on the upper stories of mixed-use buildings above the ground floor non-residential footprint. The number and space allocated to dwelling units that are located in mixed-use buildings are regulated by the equation for minimum and maximum FAR (*see 7. above.*) and shall not be included in the count of maximum and minimum number of permitted residential dwelling units (*see 5. above.*)
9. Adjustment to Transect Area and Location: The approved location and acreage within the SD15 Transect is as indicated on the adopted Uptown North Orange TND Transect Map Diagram (Exhibit 4.1 in Section 4). Minor adjustments to the transect boundary and acreage may be approved by the Community Development Director subject to demonstration of need by the applicant based on final site plan, civil, engineering or geophysical determinations. Any variation to the size of the Sd15 Transect that increases or decreases the gross transect area by $\geq 10\%$ shall be subject to review and approval by the Planning Commission. Any change in transect area and acreage shall be documented by the applicant on a revised Transect Map Diagram. If approved, the revised Transect Map Diagram shall govern.

Parking

1. The TND District encourages creative parking strategies that are aimed at balancing gross parking requirements for the project with opportunities for shared parking, on-street parking, phasing of parking, and coordination with environmentally responsive, low impact development techniques. This objective may be pursued with at the time application for individual site plan and subdivision plat approval (*see 9. and 10. herein below.*)
2. On-street parking is permitted in accord with individual street sections included in this Code of Development and as may be subsequently approved with final site plan and subdivision plats for individual projects.
3. In addition to on-street parking, off-street parking shall be provided on the individual lot subject to the zoning permit application. Parking on private property may be accessed via a driveway or drive aisle directly from the public street on which the lot fronts or via an approved alley located within the private access easement. Off-street parking access from a public street is subject to approval of a final site plan and subdivision plat.

4. Garages serving single family detached residential dwellings accessed from a public street frontage must be set back a minimum of 15' from the front building line as established for the residential unit by the SFD/Duplex Lot Development Plan.
5. Residences located in the upper levels of mixed-use buildings shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces. Not more than seventy-five percent (75%) of the required residential off-street spaces may be located within a garage or parking structure.
6. Townhouse and single family detached residences shall provide two (2) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage or parking structure.
7. Freestanding multi-family residences shall provide one and one-half (1.5) off-street parking spaces per dwelling unit. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. herein below.*) Not more than fifty percent (50%) of the required residential off-street spaces may be located within a garage or parking structure.
8. Commercial retail, office, and other non-residential uses within the transect shall conform to the parking requirements of the Town of Orange. As an option, the applicant may submit a parking impact study to justify fewer parking spaces (*see 9. and 10. hereinbelow.*) The required commercial and other non-residential off-street spaces may be located within a parking structure, subject to final site plan and plat approval.
9. Applicants are encouraged to evaluate the benefits of shared parking and other parking reduction opportunities, where appropriate. The procedure for this is through the submission of a parking impact study. If the applicant elects to establish parking requirements based on a parking impact study, the study shall be subject to review and approval by the Planning Commission. The applicant shall prepare the parking impact study employing contemporary research and parking projection techniques, including studies prepared by the Urban Land Institute.

The parking impact study for a given project shall address: (a) planned and existing on-street parking supply, (b) shared-parking opportunities with other planned and existing residential and non-residential development projects, (c) mixed-use parking relationships, (d) location of project within the transect, (e) development phasing, (f) coordination with runoff reduction techniques, and (g) demonstration of total parking adequacy to meet the marketplace demands of the transect and project. The project-specific scope of the parking impact study shall be established at a meeting with the Community Development Director. The parking impact study shall be prepared by a qualified professional.

10. The parking impact study shall be submitted to the Community Development Director for review and approval at a minimum of thirty (30) days prior to submittal of the final site plan or subdivision plat for the project. Upon review, the Community Development Director shall provide written comment to the applicant which shall be reflected in the final site plan.

Upon review of the parking impact study and the subsequent the review of the final site plan or subdivision plat, the Planning Commission may act to approve, deny, or amend with appropriate conditions the applicant's request for a reduction in the number of parking spaces or other aspects of parking requirements that would otherwise be required by the application of Town parking standards.

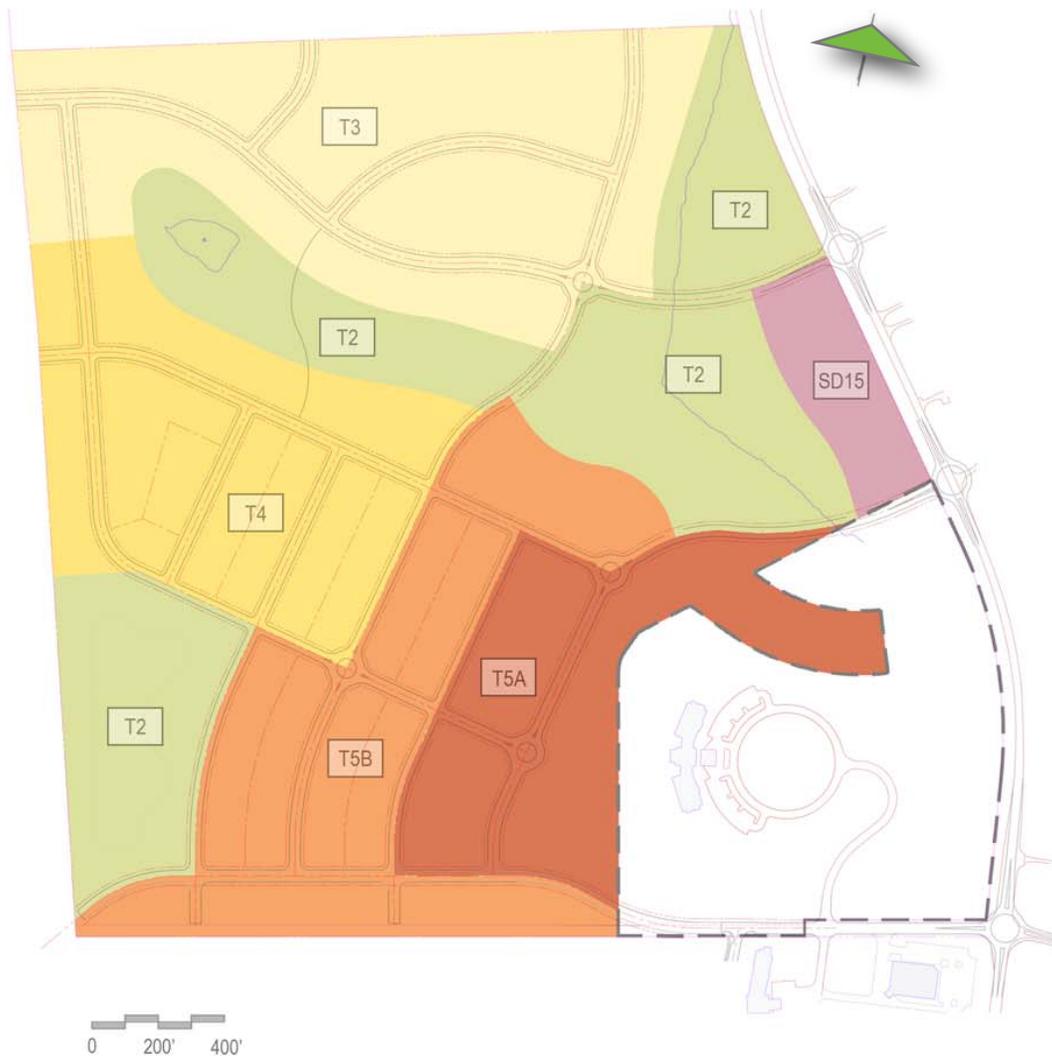
Utility Requirements

1. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public water system.
2. All lots that contain a permitted building, and all such buildings thereon, shall be served exclusively by a public sewer system.
3. All utilities that serve a structure shall be underground. Temporary construction shall be exempted from this requirement.

Special Administrative Standards: Modifications

1. The Town recognizes a process by which to obtain "major modifications" and "minor modifications" to the TND District transect zone standards contained herein. Modifications to the transect zone standards that are deemed "major modifications" by the Community Development Director, in consultation with the Town Attorney, shall require the applicant to request a formal amendment to the approved zoning of the TND District. An amendment to the zoning shall be processed by the Town in the same manner as an original zoning map amendment.
2. Modifications to the approved transect standards that are deemed "minor modifications" by the Community Development Director, in consultation with the Town Attorney, that are not otherwise delegated hereinabove to the Community Development Director may be approved by the Planning Commission upon submission of a statement of justification for the modification and the presentation of satisfactory alternatives by the applicant.
3. The Planning Commission shall consider the recommendation of the Community Development Director and other qualified sources in consideration of any minor modification from the approved transect standards hereinabove.
4. Requests for minor modifications from an approved transect standard shall be heard at a regularly scheduled meeting of the Planning Commission.

Section 6:
**Single Family / Duplex Residential
Lot Development Plan Requirements**



Round Hill/UNO-TND District

The Town of Orange

Section 6:

SINGLE FAMILY/DUPLEX RESIDENTIAL LOT DEVELOPMENT PLAN

A. Purpose and Intent

Uptown North Orange/Round Hill is a Traditional Neighborhood Development comprised of a series of transects, each of which permit a range of uses, densities, and building orientations. The regulation of single family detached and duplex housing within the transects requires an additional level of coordination for lot development activities to ensure consistency with the (1) approved TND district zoning regulations, (2) neighborhood design standards, (3) subdivision and easement plats of record, and (4) public improvements plans for streets, utilities and other infrastructure within the development. To achieve this objective, a residential lot plan (hereinafter “plan”) shall be required to accompany a zoning permit, building permit, and/or other residential use application for the following:

1. single family detached residences, two-family or duplex residences.
2. accessory buildings or apartments, accessory uses for (1) above.
3. private garages (detached and attached) for (1) above.
4. rental uses of a portions of a single family detached residential dwelling.
5. on-site parking areas or other lot improvements, including swimming pools, outdoor lighting, and fencing, for (1) above.



Graphic exhibits are illustrative and are intended to provide examples of the quality and character of future Round Hill land uses.

The plan shall provide necessary and sufficient information to document that:

- (1) the siting and use of the intended structure(s) is in conformance with the approved subdivision plat and public improvements plans for the approved transect and subdivision in which the lot(s) is located,
- (2) site grading, landscaping, stormwater drainage and runoff control for the subject lot meet or exceed requirements for residential lot development,
- (3) easements and rights of way, yard and setback requirements, transect design guidelines, and other appropriate site planning initiatives are incorporated into the lot development plan,
- (4) the siting and use of the intended structure(s) will not adversely impact adjoining properties,
- (5) architectural and landscape design are in conformance with the UNO/Round Hill TND District zoning regulations and supporting design guidelines,
- (5) environmental management improvements and erosion and sediment controls are provided to protect adjoining properties and public rights of way, and



B. Town Action on Residential Lot Development Plans

The plan shall be reviewed and acted upon by the Community Development Director within ten (10) working days upon receipt of the plan and a formal application for a zoning permit. In the event of denial by the Community Development Director, the applicant may petition the matter to be heard by the Planning Commission at their next regularly scheduled meeting. At that meeting, the Planning Commission shall act to either approve, deny, or amend the plan.



C. Preparation and Submission of Residential Lot Plans

The applicant or owner of any residential lot plan shall submit five (5) copies of the plan on a sheet not to exceed 11” x 17”, unless otherwise approved by the Community Development Director. The contents of the residential lot plan shall include the following:

1. Address and tax map reference number of lot; location map within the Round Hill development, deeded name of subdivision plat, and deed reference.
2. North arrow; dimensions of the lot drawn to scale of 1” = 10’ or greater, with meets and bounds of lot, or as otherwise may be approved by the Community Development Director.

3. Location and dimensions of both the existing structure(s) and the structure(s) to be erected on the lot. Location of adjoining structures shall be provided on lots that have previously been developed.
4. Geotechnical evaluation and certified engineering design for building foundation for lots which contain shrink/swell soils.
5. Dimensional setbacks from property lines to any structure or improvement, including porches, decks, stairwells, garages, swimming pools, accessory uses, etc.



6. Square footage of lot per recorded subdivision plat.
7. Easements and improvements contained within the lot or physical improvements which impact the lot or contiguous lots.
8. Proposed finished lot grading shown with two foot (2') contour intervals spot elevations.
9. Certification of public domestic water and sewer availability.
10. Location and design of all required off-street parking, parking access, and driveways, where applicable.

11. Location of all existing natural or man-made drainage channels and storm sewer facilities, as well as location of proposed drainage improvements, to ensure the adequate conveyance of stormwater runoff on and through the property.
12. Location and boundaries of any existing stormwater management improvements, water features, and 100 year floodplain limits that impact the subject lot(s).
13. Location of utility service connections to public water and sewer, including meter placement for domestic water, sanitary sewer lateral and cleanout locations and invert as-builts, and transformer placement for electric utilities.



14. Location and design of driveway improvements, including provisions for shared driveways, alleys, and lanes, where approved for construction.
15. Location of temporary construction entrance to lot, if required.
16. Limits of clearing and lot disturbance, including the location of existing trees in excess of 6" in diameter which are proposed to be removed outside the construction footprint.
17. Location and design of erosion and sediment practices to protect adjoining properties, rights of way, and utility infrastructure during the construction period.

18. Landscape plan for the subject lot.
19. Architectural plans for the proposed dwelling or other improvements.
20. Name, address and telephone number of plan preparer and lot owner. Certification of the lot development plan by a professional engineer, surveyor, architect or landscape architect licensed in the State of Virginia that is qualified to undertake the preparation of the requirements of this section.



Section 7:
Architecture and Landscape Guidelines



Round Hill/UNO-TND District

The Town of Orange

Round Hill Architecture and Landscape Guidelines

Introduction to the Architectural Guidelines

Article 6-10.80 of the Town of Orange’s TND District application requirements establishes certain information required to be submitted as a part of the rezoning. It is the intent of the Owner/Developer to guide the implementation of high quality architecture and landscape forms in conjunction with the establishment of a formal Design Review Committee for Round Hill. Inasmuch as the Town has no provisions for regulating architectural design on the subject property except via adopted statutory mechanisms such as historic district or entry corridor overlay design review districts, the Owner/Developer shall assume this responsibility. Acknowledging that architectural preferences are subjective, the Town has recognized the value of the Design Review Committee with the adoption of the TND District (January, 2010). The architectural and landscape architectural guidance documents and graphics (non-regulatory) of the Uptown North Orange-Code of Development shall be employed by the Design Review Committee for this purpose.

The following is the Owner/Developer’s objectives and the Design Review Committee approach to ensure that Round Hill architectural improvements are fully integrated and coordinated in the best sense of traditional Virginia design. *This section is non-regulatory and for guidance purposes only. Photographic exhibits are for illustrative purposes only in representing the desired general theme and quality for architectural massing within the community.*



First, it is recognized that a community’s vision for “appropriate architecture” is extremely difficult to quantify, with the number of opinions as varied as the number of residents who live there. For good reason, every homeowner has a unique understanding of their family’s living requirements and the organization of architectural spaces necessary to satisfy their own “sense of place”. However, the broader vision for architecture within a TND community often requires a common design vocabulary if that community is to enjoy the aesthetic and spiritual benefits that inure from a visually attractive and appropriately scaled living environment. How does this concept apply to Round Hill without “socializing” the look and feel of the community or, from a zoning standpoint, restricting or dictating architectural choice? There is much debate on how this question should be answered. However, the TND Districts establish certain expectations for individual and community architectural guidelines, while stopping short of legislating them.



The following section establishes the Owner’s vision and general guidelines for Round Hill architectural design. The design objective is to focus on the creation of a complementary, yet dynamic, set of architectural and streetscape interrelationships. It is recognized that, because of the diversity of uses and their density, scale, and function, the individual buildings cannot all come in the same “wrapper.” On the other hand, each building design should reflect the streetscape objectives of the TND Districts while attempting to reflect the rich historical character, traditional architectural elements, and environmental character of some of Orange’s more inviting neighborhoods.

The Architectural Vision for the Round Hill TND

The guiding “vision” for the review of architecture and urban design components within Round Hill should embody the following principles:

- Round Hill shall have the “look and feel” of a traditional Virginia town, with streetscape designs seeking to emulate the historic and cultural elements found in the Town of Orange and nearby communities.
- The ambiance of Round Hill shall evolve from architectural styles, building orientations, building materials, graphic identity systems, landscaping, and a common set of site and building elements. Federal, Charleston, and Georgian interpretative stylings and vernacular designs embracing colonial or revival elements should be employed to emulate traditional Virginia architecture.
- Buildings - both residential and non-residential - shall be developed with entrances and frontage on a grid of traditional streets that bind the TND together.
- The commercial and residential buildings in the TND Center which face Radney Road should have a consistent building wall to better define the space and create a unified architectural streetscape edge.
- Buildings and other structures within the TND Center should be set at, or close to, the street edge, with a sidewalk and, in many instances, a planting zone between building walls and curbs.



- Heights of buildings should be predominantly established as low-to-mid rise, with buildings of three to five stories encouraged in the TND Center and surrounding blocks.
- Buildings within the TND Center should be predominantly brick, complemented by a mixture of painted surfaces and stucco. However, brick should not be used in instances where it does not complement architectural stylings.
- To set the TND Center apart from other urban areas, architectural emphasis should be placed on memorable roofscapes, which may be composed of accent elements such as gables, rooftop cupolas, towers, and penthouses.
- Traditional urban architectural and hardscape elements, such as awning, benches, custom streetlights, bollards, and creative signage, should be compatible and coordinated with architectural stylings in both residential and non-residential buildings.



- In key locations within the TND Center, residential uses, including apartments or condominiums above first floor commercial spaces, may be located in mixed use structures.

- Buildings throughout Round Hill should be of traditional architectural themes, but incorporating state-of-the-art building technologies with the finest design and support facilities available. Uniformly designed, architecturally monotonous track housing will not be permitted.

Design Review Process

How can architectural design standards be effectively controlled? The process must be one that is more than “*Do this! Don’t do that!*” In the Town of Orange there is no legal mechanism to dictate architectural standards within a new community nor have the Planning Commission and Town Council sought to impose such regulations on architectural tastes. However, the Owner agrees that some element of self-control for architectural design must be implemented in order to achieve community goals. In most Traditional Neighborhood Developments in the United States, Design Review Committees are established for this purpose. Given that repetitious “track housing” will not be permitted by the Design Review Committee in Round Hill, it is expected that there will be multiple home builders and architects who will contribute to the development of a range of varied and creative architectural stylings in the community. This presents a challenge to the community to internally review, guide and coordinate architectural design. Thus, in addition to the architectural regulations in the individual Transect Zones, the following architectural standards shall guide the internal Design Review Committee:

Residential Lot Size Guidelines

The overall goals of the Round Hill / Uptown North Orange TND include incorporating residential uses in a variety of types and scales. This is a key component of Traditional Neighborhood Development, creating a varied streetscape, as opposed to the repetition of a single house type or lot size as often seen in contemporary suburban tract development. To encourage such a variety, residential lot type guidelines have been established in the following chart, breaking residential lots into broad categories, ranges of lot dimensions, allowability within each transect zone, and lot mix ranges within those zones. These guidelines generally organize larger house types into the T3 and T4 transects, while maintaining a mix of types within all transects. Flexibility in lot dimensions provides further variety, and allows builders the flexibility to incorporate these general lot types into a various topographic and geometric situations.

Table 7.1 : **Round Hill / Uptown North Orange**
Residential Lot Types: Lot Size and Transect Allocation Guidelines

Lot Group	Building Lot Type	Interior Lot Size ⁽¹⁾		Transect Zone			
		Width ⁽²⁾	Depth	T3	T4	T5A	T5B
Single Family Detached	COTTAGE	36'-42'	85'-120'	Range: 0% min; 20% max	Range: 20% min; 70% max	Range: 0% min; 90% max	Range: 0% min; 90% max
	HOUSE	42'-54'	85'-120'	Range: 10% min; 40% max	Range: 20% min; 70% max	Range: 0% min; 90% max	Range: 0% min; 90% max
	VILLA	54'-66'	85'-120'	Range: 20% min; 60% max	Range: 10% min; 50% max		
	ESTATE	66'-80'	85'-120'	Range: 20% min; 80% max	Range: 0% min; 50% max		
Attached Residential & Mixed	TOWNHOUSE	16'-32'	75'-100'	Range: 0% min; 10% max	Range: 10% min; 50% max	Range: 10% min; 60% max	Range: 10% min; 70% max
	MULTIFAMILY	variable, per Site Plan ⁽³⁾	variable, per Site Plan ⁽³⁾		Range: 0% min; 40% max	Range: 60% min; 100% max	Range: 20% min; 80% max
	LIVEWORK	16'-32'	75'-100'		Range: 0% min; 20% max	Range: 10% min; 30% max	Range: 10% min; 70% max
	VERTICAL MIXED USE	variable, per Site Plan ⁽³⁾	variable, per Site Plan ⁽³⁾	Mix allocation for vertical uses not regulated by this table.			

Notes:

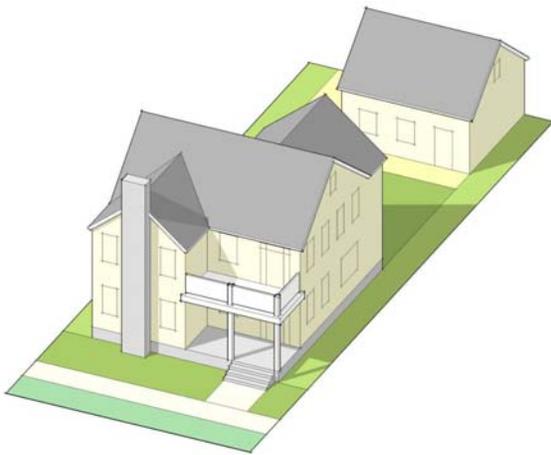
1. Lot type for cul-de-sac and pipe stem lots subject to approval of subdivision plat and lot development plan.
2. Guidelines apply to interior lots. Sideyards for corner and interior lots may be increased subject to subdivision plat and lot development plan approval.
3. Lot sizes for multifamily and vertical mixed use buildings subject to total project lot area established with final site plan.
4. Percentages represent recommend guidelines for the minimum and maximum mix of individual residential unit types within designated transect in relation to total number of planned transect residential units. No guidelines established for SD15 transect zone.
5. See individual transect zone regulations for minimum yard width, depth, and area regulations applicable to each transect zone.



Residential Cottage Lot
36-42'W x 85-120'D



Residential House Lot
42-54'W x 85-120'D



Residential Villa Lot
54-66'W x 85-120'D



Residential Estate Lot
66-88'W x 85-120'D



Residential Townhouse Lot
16-32'W x 75-100'D

Live-Work Lot
16-32'W x 75-100'D

Recommended Architectural Standards

This document describes the architectural design recommendations that will contribute to the overall design excellence of Round Hill. The intent is that the building form, silhouette, orientation, and material standards reinforce the architectural characteristics common to Traditional Neighborhood Development. Thus, the objective of architectural design criteria is to ensure a continuity and compatibility amongst all architectural elements and to maintain a high level of quality and design excellence. Further, it is a primary objective to provide for highly functional and flexible buildings which are sensitive to market considerations by being cost effective and responsive to varying tenant requirements.

The Owner will initially organize and, eventually, the residents of the community will conduct the affairs of the private Round Hill Design Review Committee to be governed within the framework of a Property Owners' Association. To ensure the adherence to design themes and concepts as well as the functional and environmental quality of the project, it would be the function of the Design Review Committee to consistently apply the design and development objectives of the Form Based Code throughout the life of the project.

Architects and urban designers of individual properties are encouraged to provide creative interpretations of traditional Virginia architecture. However, certain design and artistic fundamentals must be adhered to from block to block in order that buildings in Round Hill maintain due respect the dominant themes expressed herein. The fundamentals which the project's architects must keep clearly in their sights include: *scale, order, rhythm, balance, and proportion.*

Scale

Scale is the ratio of the size of one object as compared to the size of another. Scale relationships in architecture include: *(1) the size of the parts of a building related to that of the whole building, (2) the size of the parts of a building compared to that of the human figure, and (3) the buildings size in relation to its setting.*

The intended intimacy of the overall TND and its TND Center (Transect 5A and 5B) should emphasize pedestrian-scaled building treatments. In turn, these building treatments are to provide definition of space, interest, and articulation along the ground floor facades. The intended sense of place in the TND Center is to be provided by a distinctive set of building facades framing the overall urban design space and creating a great and inviting “urban room” for its patrons and residents.

At the same time, the scale of architectural elements within the buildings’ forms, such as doors, windows, and columns, should be consistent among the facades in the TND Center. Through the use of scale, the group of buildings may be understood as a unified whole, while the hierarchy of its parts is made visually clear.

The perceived scale of a building, as seen from a specific vantage point, may be controlled through careful siting and architectural treatment at the ground floor level. By aligning building edges in the TND Center and the higher density residential blocks, a new structure’s visual impact can be enhanced. The scale and visual impact of a building may also be controlled by breaking larger volumes into smaller components and architectural detailing.

Visual Order

Visual order is achieved through a consistency of balance, rhythm, and proportion among architectural components. It enables one to visually understand the relationships between the parts of a building as well as the relationships between distinct buildings as well. Within Round Hill, this is an essential concept for architects who will be challenged to create and maintain “order” among a wide range of both residential dwellings and non-residential buildings with varying uses, orientations, scale, densities, and massing.

The striking symmetry of traditional Virginia architectural style creates a strong sense of order. The hierarchy of scale between the central block and its dependencies, as well as the axial relationship between the main entry reinforce that order. The consistency of the windows’ and doors’ head and sill heights contributes to the composition’s overall unity.

Balance

Balance is achieved when the desired focal point of a composition is apparent. Balance is readily achieved in a symmetrical building when the focal point is located at its center. Where a building composition is asymmetrical, balance may be achieved through the use of rhythm and repetition, often combined with localized symmetry or a discontinuity in the typical patterns at the focal point itself.

Rhythm

Rhythm is fundamental to the architectural integrity of Round Hill. At its essence, rhythm is the consistent repetition of building forms or architectural components. The orderly repetition of similar architectural elements such as columns, doors, and windows contributes to the balance and order perceived in a building or group of buildings. Once a rhythmic pattern has been established, an interruption in that pattern becomes a focal point.

Within the TND Center, rhythmic patterns can be established by the regular spacing of similarly sized openings within individual buildings and blocks of buildings. Rhythms established at the ground floor level are often reproduced at the upper levels of the building. Variations in rhythm are established to provide acute visual interest in specific instances as in rooflines visible from important sight lines within and outside of the community.

Proportion

When specific scale ratios between associated architectural components are held to constant values, a proportional system results. In essence, the “proportional system” represents the means of quantification of “what feels good” about a given building, group of buildings or streetscape. Proportion, sadly, is missing from most of Northern Virginia and Central Virginia suburban commercial and residential areas. This will be remedied in Round Hill by the design review process.

Architects for Round Hill will be challenged by the Design Review Committee to ensure that proportional systems for any given building are fully integrated into the overall streetscape. With such a system, the visual relationships between the buildings within individual blocks can be more easily understood.

Proportional relationships in building elevations should include consistent window *width-to-height* ratios, consistent roof slopes at related roof forms, and uniformity in the percentage of void (window and door) area to overall area of the building elevations. Scale relationships between the Round Hill blocks and their adjacent dependencies also support the established proportional system.

Specific Architectural Guidelines

In pursuit of Round Hill's desired architectural quality, the following specific guidelines are advanced to assist designers, developers, and builders in reaching the overall goals for scale, order, rhythm, balance, and proportion:

Roof Slope

While not regulated within individual transect zones, roof slopes should mimic those found in traditional Virginia towns and cities. Such roof slopes for residential construction generally fall between 4/12 (at the flattest part of the spectrum) and 12/12 (at the steepest part of the spectrum). Within the pitch of the roof, structures may include a half story, also called a dormer story. The design of dormers should mimic the pattern of openings on lower floors, and should generally complement the overall design of windows and other features within the structure as a whole. For commercial development, even lower pitches may be used, including roofs that are entirely flat.

Façade Indentations

Indentations are recesses within the building façade, from the primary plane of the façade. Indentations are an integral part of the overall composition of the building's façade. While residential structures should often feature deep porches that shield the front door, this is not a desirable feature in commercial development, where entrances should be plainly obvious to potential customers, and facades should present a defined street wall. With this in mind, indentations within commercial facades should generally be limited to the following dimensions within the noted transects, measured in the maximum depth of the indentation, and the maximum width as percentage of the building's total frontage:

<i>T4 Transect:</i>	<i>30" max. indentation for maximum 80% of façade</i>
<i>T5A Transect:</i>	<i>18" max. indentation for maximum 60% of façade</i>
<i>T5B Transect:</i>	<i>24" max. indentation for maximum 70% of façade</i>

Landscape Design Guidelines – Introduction

One of the most distinguishing features of Round Hill will be a Traditional Neighborhood streetscape and landscape framework that is based on the historic patterns found in many of Virginia’s more attractive towns and cities. The “vision” for the TND is that it convey an image of something other than a “tract housing” development. This “vision” can be achieved via a thoughtful set of landscape and lot development design guidelines that can be applied throughout the project. The exhibits presented in this section as well as the larger-scale Illustrative Master Plan and the Streetscape and ROW Design Sections depict the planned locations of major landscape forms associated with streetscapes, buffer areas, and other landscape zones within the project. A detailed landscape design package will be presented with the final site plan for the first phase of development.

The purpose of this section is to establish the expectations, design concepts, plant material lists, and qualities of landscaping and lot development elements which are intended to be implemented throughout the Round Hill project. The areas of landscape responsibility for individual property owners will be defined herein and, subsequently, more thoroughly designed in the final site plans for each section of the project. As noted on the streetscape section graphics, the application of the urban landscape and streetscape concepts begins at Round Hill’s public gateways and continues throughout the interior neighborhoods, communicating a changing scale and landscape character as the user enters the more intimate areas of the residential neighborhoods or the more intensive areas of the activity centers closer to Radney Road and Route 15.

Building and Streetscape Relationships

The TND Districts establish a strong priority on building and streetscape relationships. As such, landscape and urban design forms are to be considered as integral elements that, in combination, establish the “texture” as well as the “form” of Round Hill. The landscape architectural theme for the major categories of streetscape elements, consisting of the public entrance gateways, the roundabout intersections, the tree-lined minor collector streets, and village-scaled residential streets, is to be implemented concurrently with the balance of Round Hill’s public improvements. The proffers address the timing of implementation of these public improvements. The development of each residential and non-residential building lot shall reinforce and add to this image by employing complementary landscape forms, patterns, and materials to create appropriate form and scale.

In general, residential dwellings and non-residential buildings should be slightly elevated above the public streets whenever possible to improve image, visibility, and site drainage. Where buildings are lower than street level, special landscape and hardscape attention should be given to the street’s edge with private lawns or civic spaces. The grading of adjacent buildings shall be coordinated to meet common facilities such as parking lots, building lobbies, driveway entrances, and motor courts at the same grade.

While not possible in every case, buildings within the same neighborhood grouping or cluster should have coordinated finished floor elevations and/or massing and details which relate the

individual buildings to each other. Berms, if applied, shall be gently curved and rolling at an appropriate scale, and artificial-looking earth “mounds” are not recommended.

Landscape and Grading Relationships

To reduce potential adverse environmental impacts, final site designs shall minimize the extent of mass grading within the community. All site grading will be done in a way that enhances the preservation of mature vegetation around the perimeter of Round Hill. A maximum side slope of 3:1 around exterior building walls will be allowed, provided that retaining walls may also be necessary in some instances to preserve existing vegetation. Best Management Practices (BMPs) shall be developed in accord with a SWM and BMP Master Plan to be prepared and submitted with the initial final site plan application.

Existing natural areas situated outside building zones will be organized into a comprehensive community landscape design program to enhance the visual character of Round Hill and its surrounds. “Tree save” areas should be incorporated into the landscape design plan to serve as “passive” settings for open space, where possible.

Transect, Streetscapes and Landscape Relationships

The final design of interior blocks within the project shall be given special urban design and landscape treatments that will respond to the particular land uses located therein. For example, the transects representing the TND Center blocks and other areas containing common landscaped areas inuring to the benefit of the community’s residents should have unique landscape features that create a “sense of place”. As depicted on the Illustrative Master Plan, open space and park land, active recreational spaces, and intersectional “gateway” landscaping will be incorporated into the final site design by the Owner. These are to be maintained by the Round Hill Property Owners' Association. The landscaping for these blocks and special areas falls into several general categories:

- **Street Tree Landscape Zones:** These consist of the street tree plantings, sidewalks, and lawn areas adjacent to the project’s major and minor public streets which are intended to be dedicated by the Owner to the Town and to the Virginia Department of Transportation. “Gateway” landscaping principles will be applied to intersections. Roundabouts will be landscaped but will respect VDOT landscape design guidelines.
- **Park and Open Space Zones:** These are isolated peripheral “buffer” areas of the project which consist of existing wooded areas along the Route 15 frontage and the northern perimeter of the property. A trail system will provide access to these areas but the primary intent is for these areas to provide adequate landscape design and, where appropriate, landscape buffering along the entrance “gateway” to the Town. The final design for

these areas will be incorporated into the site plan for the Park and Open Space areas.

- **Transitional Landscape Areas:** Transitional landscape areas consist of the areas internal to individual blocks and streetscapes, including utility easements and parking lots, which are located between the buildings and the common landscape areas. This landscape development shall be implemented by the builders and developers of individual neighborhoods, or blocks, and shall be compatible with the adjacent landscape material in color, form, and texture. The horticultural intent is to provide interesting landscape color and texture throughout the year.

Landscape architecture and lot development elements shall be sympathetic to and coordinated with the architectural scheme and context with respect to scale, color, and organization. The images on this page are illustrative of the landscape and hardscape elements that should be addressed during the final landscape design process. Where conditions dictate, buffers should be employed to separate different site-specific uses in order to obtain both a level of privacy and individualization, as well as to minimize noise pollution levels. In non-residential areas, building service areas and public utilities should be screened from view with vegetation. All screening should be integrated with existing vegetation stands and coordinated supplement landscaping wherever possible.

Plant Material Guidelines

Deciduous and Street Trees

Major deciduous trees shall be used in organized streetscape designs between the building zone and the street edge. They may be located either within sidewalk planters or within planting zones between the sidewalk and curb. Also, these street trees should be used in parking islands of surface and structured lots, and on lawns of community facilities, offices and the TND Center block retail uses, where they do not interfere with the landscape architectural design intent of the street tree plantings. Minimum size: 2 1/2" - 3 1/2" caliper.

Ornamental Trees

Ornamental trees that normally grow from 12' to 25' tall at maturity should be liberally planted for accents and visual emphasis. The flowering habits of ornamental trees are very effective when used on non-residential lawns or in the vicinity of TND Center buildings where space is adequate. Ornamental trees are most effective with a dark background provided by architecture or evergreen trees. Multiple or single stem, small scale trees may be used in small scale pedestrian locations where space is limited and an intimate feeling is appropriate. Minimum size: 6'-9' height.

Evergreen and Coniferous Trees

Evergreen trees shall be used at strategic locations for screening and buffers within the Round Hill TND and should be designed into group plantings to enhance “winter” seasonal interest, screen objectionable views of service areas and parking structures, and as a backdrop for ornamental trees. Evergreen trees shall be selected from species native to the area or species that are similar in appearance to natives. See selection list on following pages for recommended species. Minimum size: 6'-9' height.

Deciduous Shrubs

Deciduous shrubs shall be used as accents on private residential lots to create seasonal color interest. Due to their informal appearance, they should not be used in high image areas where a manicured formal image is desired. Deciduous shrubs should be maintained with a "natural" growth habitat and shall not be used as formal hedges, except in occasional streetscape settings. Minimum preferred size: 3'-4' height. These shall be selected from varieties of winter hardy shrubs commonly found in the mid-Atlantic region. They may be used where a low level screen or hedge is desired at parking lots, sidewalks or as mass plantings in the landscape; they may also be used as effective ground covers on slopes. Minimum size: 18"-24" spread.

Ground Covers

Euonymus fortunei “Coloratus” (Evergreen Wintercreeper) and native grasses are recommended for ground cover plantings fronting on the roundabouts and the non-residential properties fronting on public streets. Certified turf grass may be a substitute where warranted by design. Recommended spacing: plant 18” on center. Appropriate ground covers are recommended for use on all slopes steeper than a rise of one foot in three feet of run. Minimum size: Vines - 2 1/4" pots, Shrubs - 18"-24" spread.

Lawns

Lawns and other turf zones within the public street rights of way, residential lots, and other private development areas shall be sodded. Sod shall be used on all disturbed areas and individual lawns within block development projects not otherwise planted or in mulched beds or where existing ground cover is adequate and can be preserved. Where sod is employed, it shall be of a quality meeting the certification requirements for materials and installation practices of the Virginia Association of Turf Growers. Sodding, in combination with other methods of infiltration on individual lots, shall qualify as a site-specific BMP measure.

Plant Material List

The following plant materials are among those recommended to be employed in conjunction with final landscape plans to be submitted with final site plans and subdivision plats as well as lot development plans for single family residential uses. This list does not limit the potential approval of other species

Canopy Trees

<i>Acer rubrum</i>	<i>Red Maple</i>
<i>Acer saccharum</i>	<i>Sugar Maple</i>
<i>Chionanthus virginicus</i>	<i>White Fringetree</i>
<i>Cladrastis lutea</i>	<i>American Yellowwood</i>
<i>Fagus grandifolia</i>	<i>American Beech</i>
<i>Fraxinus americana</i>	<i>White Ash</i>
<i>Fraxinus pennsylvanica</i> “Newport”	<i>Newport Green Ash</i>
<i>Ginkgo biloba</i>	<i>Ginkgo (male)</i>
<i>Gleditsia triacanthos</i> var. <i>inermis</i>	<i>Thornless Honeylocust</i>
<i>Liquidambar styraciflua</i>	<i>Sweet Gum</i>
<i>Nyssa sylvatica</i>	<i>Sourgum</i>
<i>Oxydendrum arborium</i>	<i>Sourwood</i>
<i>Platanus occidentalis</i>	<i>Sycamore</i>
<i>Quercus alba</i>	<i>White Oak</i>
<i>Quercus coccinea</i>	<i>Scarlet Oak</i>
<i>Quercus palustris</i>	<i>Pin Oak</i>
<i>Quercus phellos</i>	<i>Willow Oak</i>
<i>Quercus rubra</i>	<i>Red Oak</i>
<i>Quercus shumardii</i>	<i>Shumard Oak</i>
<i>Taxodium disticum</i>	<i>Common Baldcypress</i>
<i>Tilia cordata</i>	<i>Littleleaf Linden</i>
<i>Zelkova serrata</i>	<i>Japanese Zelkova</i>

Ornamental Trees

<i>Amelanchier canadensis</i>	<i>Shadblow Serviceberry</i>
<i>Cercis canadensis</i>	<i>Eastern Redbud</i>
<i>Chionanthus virginicus</i>	<i>White Fringetree</i>
<i>Cornus florida</i>	<i>Flowering Dogwood</i>
<i>Cornus kousa</i>	<i>Kousa Dogwood</i>
<i>Crataegus</i> sp.	<i>Hawthorn</i>
<i>Halesia carolina</i>	<i>Carolina Silverbell</i>
<i>Lagerstroemia indica</i>	<i>Crape myrtle</i>
<i>Magnolia soulangeana</i>	<i>Saucer Magnolia</i>
<i>Magnolia stellata</i>	<i>Star Magnolia</i>
<i>Magnolia virginiana</i>	<i>Sweetbay Magnolia</i>
<i>Malus</i> sp.	<i>Flowering Crabapple</i>
<i>Prunus serrulata</i>	<i>Flowering Cherry</i>
<i>Stewartia ovata</i>	<i>Mountain Stewartia</i>
<i>Styrax japonicum</i>	<i>Japanese Snowbell</i>

Evergreen Trees

Ilex opaca
Pinus nigra
Pinus strobus
Pinus thunbergi
Tsuga canadensis

American Holly
Austrian Pine
White Pine
Japanese Black Pine
Canadian Hemlock

Deciduous Shrubs

Berberis sp.
Cotoneaster sp.
Euonymus sp.
Forsythia x intermedia
Fothergilla gardenii
Hamamelis virginiana
Jasminum nudiflorum
Potentilla fruticosa
Pyracantha coccinea 'lowboy'
Rhododendron sp.
Rhododendron sp.
Viburnum sp.

Barberry
Costoneaster
Euonymus
Forsythia
Dwarf Fothergilla
Witchhazel
Winter Jasmine
Shrubby Cinquefoil
Firethorn
Azalea
Rhododendron
Viburnum

Evergreen Shrubs

Ilex sp.
Ilex glabra "Shamrock"
Juniperus sp.
Kalmia latifolia
Mahonia aquifolium
Pieris japonica
Taxus sp

Holly
Inkberry
Juniper
Mountain Laurel
Oregon Grapeholly
Japanese Pieris
Yew

Groundcovers

Ajuga reptans
Euonymus fortunei "Coloratus"
Hedera helix
Hosta sp.
Hypericum sp.
Liriope muscari
Liriope spicata
Pachysandra terminalis
Vinca minor

Buglewort
Purple Wintercreeper
English Ivy
Hosta
Hypericum
Liriope
Spreading Liriope
Japanese Spurge
Periwinkle

Plant Materials Standards

All plant materials to be used in Round Hill shall be nursery grown in accordance with the highest standards of horticultural practices and should be grown under climatic conditions similar to those of the site for at least two years. Plants shall be typical of their species or variety and should have normal growth habits. Plants shall be free of disease and shall have healthy, well-developed root systems. All plants shall conform to the American Standard for Nursery Stock as published by the American Association of Nurserymen. The use of a plant bed edging or mowing strip is recommended. The following materials are acceptable: steel (black), concrete, stone, and brick. Mulch shall be dark, well rotted bark from a pine, fir, or hardwood source. Maximum dimension for chunk type should not exceed 1.5 inches. Maximum length for shredded type should not exceed 4 inches.

Landscape Maintenance

A Property Owners' Association shall be formed for the purpose of maintaining common areas that are situated outside the areas of landscape maintenance responsibility of the Town. It is recommended that individual owners of single family residences shall be responsible for executing a landscape maintenance program for landscape areas within their home sites. The program shall include the following and be coordinated with the maintenance of common areas:

- Prune trees and shrubs as necessary to maintain an attractive shape; remove dead branches and provide clearance for vehicles and pedestrians.
- Replace dead and disfigured plant material immediately with plants of equal size except in environmentally sensitive areas at the discretion of the Property Owners' Association.
- Fertilize, mulch, water, and weed plant beds. Apply insecticides and fungicides as necessary to maintain plant vigor and appearance. Environmentally-friendly fertilizers are recommended.
- Lawns shall be watered, mowed, and maintained in a dense, weed-free condition. Lawns shall be edged adjacent to paved surfaces.
- Remove fallen leaves, branches, paper, and other debris from lawn, shrub, and pavement areas.

Landscape Quality Guidelines

The following general standards and guidelines shall apply to the planning, design, installation and maintenance of all landscape improvements, site plan improvements, urban design standards, screening and buffering measures, and related site development practices for Round Hill TND, and these shall be further regulated by the Code of Development and the ordinances of the Town:

- A. A landscape plan shall be required for all uses requiring a subdivision or site plan as well as other residential development plan applications. Landscaping, screening, and buffering for any subdivision and site development activity shall be guided by this Code of Development as well as generally accepted site and landscape design principles.
- B. Landscape design plans shall maximize the preservation of existing trees and minimize the disruption of established landscape materials, employing preservation and protection criteria provided in the Virginia Erosion and Sediment Control Manual and the State's Urban Best Management Practices Handbook.
- C. The quality and type of all new plant materials installed on a site shall be in accord with the specifications of the American Association of Nurserymen, provided that the transplanting of trees and shrubs may be done in accordance with accepted horticultural and forestry practices. The planting and placement of trees shall be done in accord with the standardized landscape specifications of the Virginia Society of Landscape Designers and the Virginia Code of Development of the American Society of Landscape Architects.
- D. The property owner, developer and or successor in title to a given property shall be responsible for the perpetual maintenance as well as timely repair and replacement of all landscaping, buffers and screening as required by this Code of Development. A failure to adequately maintain landscape improvements in a healthy state and to keep such improvements free of litter, refuse and debris shall be deemed a violation of Design Review Committee requirements.
- E. Landscape planting, maintenance and repair regulations also shall apply to all land in common open space, active recreational areas, or other lands not in lots in residential subdivisions. Property owners' documents should incorporate such requirements.
- F. Landscape plans to accompany any Round Hill TND site plan or subdivision plat shall be prepared by a qualified landscape designer, nurseryman or horticulturist in association with a Professional Engineer, Architect or Surveyor of record (certifying the plan or plat) or shall be prepared and certified by a Certified Landscape Architect licensed to practice in the State of Virginia.
- G. The landscaping and buffering standards hereinafter established provide minimum guidelines for landscape architecture of commercial, industrial, institutional and residential properties. Refer to the Streetscape Sections for graphic examples and visual guidelines.

Streetscape and Canopy Planting Guidelines

The streetscape landscape plan to be submitted to the Design Review Committee shall demonstrate that it meets the following canopy and tree preservation standards with any application for a residential or non-residential development project within Round Hill TND:

- A. The developer shall make strong efforts to preserve and protect existing trees in the design and development of individual sections of Round Hill TND and, where infeasible to preserve trees, shall replace those trees that may need to be necessarily removed during such development.
- B. As a general guideline, the subdivision plat or site plan for any subdivision or site development section within the Round Hill TND should include the planting and/or replacement of trees on the site to the extent that, at twenty (20) years of growth, minimum tree canopies or tree covers within that section of development will be provided in accord with the following standards:
 1. The TND Center: Ten percent (10%) of the total site area shall be covered in tree canopy.
 2. TND Mixed Use Residential Areas: Ten percent (10%) of the total site area shall be covered in tree canopy.
 3. Block B Residential Areas: Fifteen percent (15%) of the total site area shall be covered in tree canopy.
 4. Block A Single Family Residential Areas: Twenty percent (20%) of the total site area shall be covered in tree canopy
 5. Off-Site Planting Option: If the full canopy guideline cannot be met by on-site plantings within each section of development, this standard may be satisfied by off-site plantings or right of way plantings, where approved by both the Town and the Design Review Committee.
- C. All landscape materials shall conform with the following minimum size or height (at date of planting) standards:
 1. *Deciduous shade trees:* 2.5" caliper
 2. *Street trees:* 2.5" caliper
 3. *Ornamental and understory trees:* 6' height
 4. *Coniferous trees:* 6' height
 5. *Evergreen shrubs:* 18" spread or height
 6. *Deciduous shrubs:* 18" spread or height

- D. “Tree canopy” or “tree cover” is defined to include all areas of coverage by trees and plant materials, including street trees, provided that all such trees and plant materials exceed six (6) feet in height at date of planting.
- E. All landscape materials shall be selected and sized in accordance with hardiness rating and growth habit appropriate for the intended placement of materials. All landscape materials should be well branched and well formed, vigorous, healthy and free from disease, sun and wind and insect damage and shall have healthy and unbroken root systems.
- F. Tree canopy requirements may be reduced on a case-by-case basis by Community Development Director during the site plan and/or subdivision plat approval process where it can be clearly demonstrated by the applicant that either (a) the reduced canopy achieves the intended landscape design objective through a combination of alternative landscape architectural and landscaping techniques or (b) where the characteristics property is such that the canopy coverage would not be effective and other methods of landscaping provide equal and adequate design responses. The stipulated sizes may be modified based on property conditions and site design requirements.
- G. The removal of any trees which exceed 12” caliper (dbh, breast height) shall be justified by the applicant in cases where modifications to the layout and design elements of the site plan could protect such trees without impairing the function and economy of the intended use.
- H. Existing trees which are to be preserved may be included to meet all or part of the aforementioned canopy requirements; provided that the landscape plan identifies such existing trees and that these trees meet standards of desirability, hardiness and life expectancy as established by the Town.
- I. In cases where the application of the screening and/or parking lot landscape requirements cited hereinafter may be less than the above minimum canopy guideline, the final canopy requirement, tree placement, landscape materials type, and quantity of landscape materials shall be determined as a part of the Design Review Committee process, provided that nothing herein shall restrict the applicant from exceeding the minimum canopy requirements, if desired.
- J. No landscaping materials shall be installed which interfere with minimum transportation site distances for public and private streets. The landscape plan shall depict sight distance alignments and the location of sight easements.

Parking Lot Design and Landscaping Guidelines

The landscape plan for the T5 Transect to be submitted to the Design Review Committee shall meet the following canopy and tree preservation standards with any application for a residential or non-residential development project within Round Hill TND:

- A. T5 Transect Parking Lots Adjacent to Lot Lines: For the T5 Transect, private off-street parking lots and private access aisles adjacent to lot lines and which are subject to site plan or subdivision approval for uses within the TND, the following landscape regulations shall apply:
 - 1. Where a T5 parking lot (or a private driveway providing access to a parking lot or building entry) abuts a property line not common with the right of way of a street, a landscaping strip of not fewer than five (5) feet in width shall be located between the parking lot and the abutting property line.
 - 2. A minimum of one tree for each forty to forty-five feet (40'-45') of contiguous property line shall be planted in the T5 landscaping strip. This planting shall be in addition to any planting within a designated buffer area and in addition to any planting within six (6) feet of a building or structure.
 - 3. Where appropriate, shrubs and ground covers shall be provided within the landscaping strip to establish an enhanced low level visual buffer between the adjoining properties. Landscape strips shall be sodded unless otherwise approved by the Community Development Director.
 - 4. At maturity, these shrubs and other ground covers shall be two to three feet in height. Landscape plans are encouraged to incorporate gently sloping earth berms, where appropriate, into the buffer strips.
- B. Parking Lots Adjacent to Public Streets: For private off-street parking lots and private access adjacent to public streets and which are subject to site plan or subdivision approval for approved land uses, the following landscape regulations shall apply:
 - 1. Where a parking lot (or a private driveway providing access to a parking lot or building entry) abuts a public right of way for a Town or VDOT street, a landscaping strip of not fewer than five (5) feet in width (not including sidewalk) shall be located between the parking lot or private driveway and the right of way line.
 - 2. A minimum of one tree for each forty to forty five feet (40'-45') feet of property line common with the public right of way shall be

planted in the landscaping strip. This planting shall be in addition to any planting within a required zoning district buffer yard and in addition to any planting within six (6) feet of a building or structure.

3. Where appropriate, shrubs and ground covers shall be provided within the landscaping strip to establish an enhanced low level visual buffer between the parking lot and the public right of way. All landscaping strips shall be sodded with certified turf grass.
 4. At maturity, the shrubs shall be a minimum of two to three feet in height. Taller shrubs are permitted where sight distances are non impaired. The landscape design for such shrubs shall also serve to direct and control pedestrian access into parking lots.
 5. Landscape plans are encouraged to incorporate earth berms, where appropriate to the TND master plan objectives, into the right of way landscaped buffer strips.
- C. All private, off-street parking lots of twenty (20) or more spaces shall contain within the interior of the parking lot not fewer than one tree for every ten (10) parking spaces or fraction thereof. Such trees shall be reasonably dispersed throughout interior of the parking lot in accord with good landscape and urban design practices. All plant materials shall be of a variety and size which can be used to meet the tree canopy cover requirements herein above outlined.
- D. Landscaping located within the interior of parking lots shall be contained within sodded “planting islands” with raised medians. Planting islands which are located parallel to the long dimension of a parking spaces shall have a minimum width of nine (9) feet and shall be designed to permit vehicular doors to open fully without impacting plant materials. A median (or island) shall be constructed to separate no more than twelve to fifteen (12-15) contiguous perpendicular spaces for single-loading bays and twenty-four to thirty (24-30) for double-loading bays. A planting island with raised median (with raised curbing or curb and gutter) shall be constructed to terminate the end of any perpendicular parking bay and to provide adequate separation from an adjacent private street, access aisle or travelway.
- E. Landscaped “planting islands” (located such that parking spaces are on opposing sides of the planting island) shall be developed in parking lots meeting the following criteria:
1. The total size of the parking lot exceeds one hundred fifty (150) total parking spaces,

2. Parking lot layout incorporates three or more double-loaded or single-loaded parking bays which are contiguous and parallel to each other, and the requirements of Paragraph 4 above.
 3. Planting islands which are designed to be perpendicular to the parking bay shall be constructed for every third parking bay.
 4. Planting islands shall have a minimum width of six (6) feet to allow for bumper overhang and shall provide adequate width for the growth and maintenance of the intended landscape materials to be planted therein.
- F. The primary landscaping materials used in parking lots shall be trees which provide shade or are capable of providing shade at maturity. All landscape islands shall be sodded. Sod, shrubs and other live planting material shall be required to complement the primary, tree landscaping.
- G. The landscaping shall be dispersed throughout the parking lot, with interior dimensions of any planting area (i.e. interior parking median) sufficient to protect and maintain all landscaping materials planted therein.
- H. The type and method of parking lot landscape irrigation shall be described in the site plan if any type and method is to be included in the design.
- I. Not applicable to single family detached residential lots: The landscape provisions for commercial parking lots of this Code of Development are not intended to apply to off-street parking spaces or private driveway access to such off-street parking spaces for individual single family residential dwellings.

Transitional Screening

The landscape plan to be submitted to the Design Review Committee shall meet the following canopy and tree preservation standards with any application for a residential or non-residential development project within Round Hill TND:

- A. All outdoor storage and loading areas shall be screened from all public streets and adjacent residential properties. An opaque screen with a minimum height of six (6) feet of landscape materials, architectural walls, opaque fencing or other opaque material shall be used.
- B. No landscape materials or screening shall be installed which interfere with minimum transportation site distances. The landscape plan shall depict sight distance alignments and the location of sight easements in accord with transportation design standards.

- C. The applicant's architect, engineer, landscape architect or qualified landscape designer shall submit a plan and program for transitional screening during the final site plan phase to determine the most appropriate buffer and screening design concepts, tree placement and suitable species of trees to be used for screening purposes.

Section 8:
Signage and Infrastructure Guidelines



Round Hill/UNO-TND District

The Town of Orange

Round Hill Signage & Infrastructure

Introduction

The signage guidelines for the Round Hill TND shall encourage creativity in sign design. Signs within the Round Hill TND shall meet all Town requirements and ordinances unless specifically waived or otherwise modified by action of the Planning Commission upon recommendation by the Community Development Director at the time of site plan or subdivision plat approval. The sign guidelines incorporate measures which have been structured to achieve a coherent image within the community. The primary area for sign coordination will be in the Town Center transects (Transects 5A and 5B.)

As currently regulated by the Town's sign requirements, signage shall be kept at a minimum in the residential and open space transects. Although the signs will physically occupy little space, their visual impact on the overall image of the Town Center will be very important. For this reason, the signage regulations have been structured to provide a consistent image along the public streets while encouraging more flexibility with building signage and within the pedestrian areas accessing commercial areas.

Town Center transect signage concepts and designs shall be submitted to both the Town and the Design Review Committee for approval prior to installation. All signs must conform to the signing provisions of the Town Zoning Ordinance in addition to the criteria in this section. The Town Center signage system has been divided into the following signage categories -- Free Standing, Building Identity, Building Mounted Signs, and Small Regulatory Signs. *The photographic exhibits contained herein are provided for illustrative purposes only.*

Freestanding Signs

Because of frequency and exposure, this sign type is the most important of all recommended signage within the Town Center transects. While one free standing building identity sign is permitted for each detached commercial or mixed use building, careful design and placement practices should be employed in the use of any free standing signs within the retail and civic spaces in the Town Center. Free standing signs are acceptable for commercial and residential uses in Round Hill as well as for neighborhood identification in the TND residential transects.

A standard, two-faced sign is recommended due to ease of visibility. This standard should be used as the only free standing building identity sign. Although the sign housing will be standardized, the graphic parameters will be designed to permit maximum design flexibility on the sign faces by the individual user. The services of a graphic designer are strongly recommended in laying out the sign face. All signs are located at approximately the same locations (to be set by developer) relative to the entrance drives. This consistent sign placement in conjunction with the standard sign housing creates a subtle but effective unified image of the entire site.

Building Identity Sign

One tenant directory sign may be used per building. This sign is to incorporate the standard type sign housing at the prescribed location. It should be designed from the same materials as the building identity sign to ensure compatibility. These signs shall be limited to identifying the name of the building and/or the individual enterprises located therein, trademark or identifying symbol or any combination thereof. These signs are not intended as vehicles for advertising. No other free standing signs, other than directional signs, will be permitted.

Building Mounted Signs

In addition to the single free standing signs, building mounted signs will be permitted. Fundamental to Design Review Committee approval is that the building mounted signs are compatible with the architectural design of the building. In general, building mounted signs should not be visible from adjacent residential neighborhoods.

Building mounted signs may be located on the surface of Town Center building facades in accordance with the following specific guidelines:

- Signs may be located in the horizontal base band of the building. This area is defined as extending from the ground level to the floor slab of the second level of the building, or an area not exceeding twenty (20) feet upward from the ground level, whichever is fewer. Only the building facade containing the major entrance to the building may be used for signing in the base band.
- Signs may be located in the top horizontal band of a building. This area is defined as the top horizontal band containing the uppermost two occupied levels of the building. In the top band, unlike the base band, signs may be located on any of the building's facades, as long as the total permitted signage area is not exceeded. Only one (1) sign per facade is permitted in the top band.



- No signs shall be located on a structural extension or other architectural accessory and/or decorative building feature.
- No part of any sign shall extend above or beyond the perimeter of the building parapet or roof.
- Signs may be located on the vertical face of a marquee, but no part of the sign shall extend above or below the vertical face. The bottom of a marquee sign shall be no fewer than ten (10) feet above a walkway or grade, at any point.



The total sign area for all Town Center building mounted signs cannot exceed one and one half (1-1/2) square feet of sign area for each of the first one hundred (100) linear feet of building frontage plus one square foot of sign area for each linear feet of building over one hundred (100) feet of frontage. The “building frontage” is the facade which is architecturally designed as the front of the building and which contains the main entrance for use by the general public. However, no one sign shall have a sign area in excess of thirty-six (36) square feet in the building base band and eighty (80) square feet in the building’s top band.

Except as qualified below, the area of a sign shall be computed as the entire area within a single continuous rectilinear perimeter of not more than eight (8) straight lines enclosing the extreme limits of writing, representation, emblems, or a figure of similar character together with all material, color, or lighting forming an integral part of the display or used to differentiate the sign from the background against which it is placed.

The area of building-mounted signs composed of individual letters and/or symbols shall be computed in accordance with one of the following methods:

- If the space between the proposed individual letters and/or symbols is smaller than the width of the largest unit, the computation shall be made in accordance with a strict application of the provisions above.
- If the space between the proposed individual letters and/or symbols is greater than the width of the largest unit, the computation may be based on the total area within rectangular enclosures surrounding each individual unit.
- Maximum lettering and image size is 24” on the building’s base band. In the building’s top band the maximum image or lettering height shall be no larger than 1/2” of the letter or logo height per one foot of building height measured from final grade to roof top.

Illuminated signs of the following characteristics are permitted:

- Individual dimensional word or metal backlit (“halo-effect”) letters. The illumination behind such letters should be white neon unless approved by the Design Review Committee.
- Internally-illuminated channel letters with opaque metal sides and white translucent acrylic faces.

Non-illuminated signs of the following characteristics are permitted:

- Individual dimensional letters or gold or silver leafed letters applied directly to glass or other signing surfaces with other colors to be approved by the Design Review Committee. Painted wood or painted metal is encouraged.
- Letters formed from thin metal plate or similar materials and pin-mounted at a distance approximately 1 inch from the signing surface. The face of such signs should not extend more than three inches from the signing surface.
- Boxed or cabinet-type signs will be permitted on the buildings base band only. This sign type must be totally recessed and the sign background must be opaque and darker than the image.
- The following types of illumination are prohibited: (1) Any arrangement of exposed tubing or a series of lights in rows, strings, patterns affixed to any portion of a building or structure, 2) Any sign displaying flashing or intermittent lights, or lights of changing degrees of intensity of color or moving copy (exclusive of temporary Christmas and seasonal holiday lighting).
- Signs shall be limited to identifying the name of the building and/or the individual enterprises located therein, trademark or identifying symbol or any combination thereof.

- Signs in the top horizontal band shall be limited to the logo, logo type, or initials of the enterprise.

Small Regulatory Signs

Small regulatory signs shall incorporate common design themes and complementary textures and colors throughout the Town Center, public parks, and open space areas. These signs are defined as the following:

- Small signs displayed for the direction or convenience of the public, such as signs which direct traffic or identify the location of freight entrances, public and private parking areas, and access to park and open space areas. These signs should not exceed two (2) square feet in area.
- Signs warning the public against trespassing, swimming, or the like. These signs should not exceed four (4) square feet in area and should be posted at eye level.
- Small signs, above grade, which identify parking for the handicapped as required by the Town of Orange Zoning Ordinance. This sign type shall not exceed one and one half (1 1/2) square feet in area.

SD 15 Transect Gateway Improvements and Related Signs

In accord with the in-kind improvements referenced in the Proffer Statement, “Gateway” landscape and hardscape improvements shall be installed on the Round Hill property at its (a) major entrance on Route 15 and (b) east entrance on Radney Road. Signage, landscape, hardscape, street pavements, pedestrian improvements, fencing, decorative lighting designs and plans for other related amenities shall be submitted with the first final site plan for the project.

The gateway improvements shall be maintained by the Round Hill Property Owners’ Association. The master plan for the “gateway” improvements, including specialty signage and sign standards, shall be reviewed and approved by the Design Review Committee prior to its submission to the Town for approval with the first final site plan. Signage details shall be consistent with those outlined herein above and, further, shall meet all requirements of the Town of Orange.

Utility and Public Infrastructure

Service mains and essential public infrastructure shall be constructed by the developer and located within public street rights of way and other utility easements. To be constructed underground, infrastructure includes water, sewer, storm drainage, telephone, electricity, cable, and gas. Utilities and infrastructure shall be sized in accord with capacity analysis to be submitted with approved final site plans and subdivision plats. In order to achieve the desired Traditional Neighborhood Development site organization and streetscape forms, the Town recognizes that water, sewer and drainage improvements should be located within the public street section.

The graphic on the following page depicts the schematic plan for the preferred location of the key public utilities. This plan can be readily adapted to achieve compatibility with each of the public streetscape sections and their appropriate location as described by the Thoroughfare Map Diagram.

Service mains for water sewer and drainage shall be bonded prior to site improvements and, upon inspection, shall be dedicated to the Town for ownership and maintenance. The developer shall be responsible for the coordination and construction of service laterals to connect into these systems.

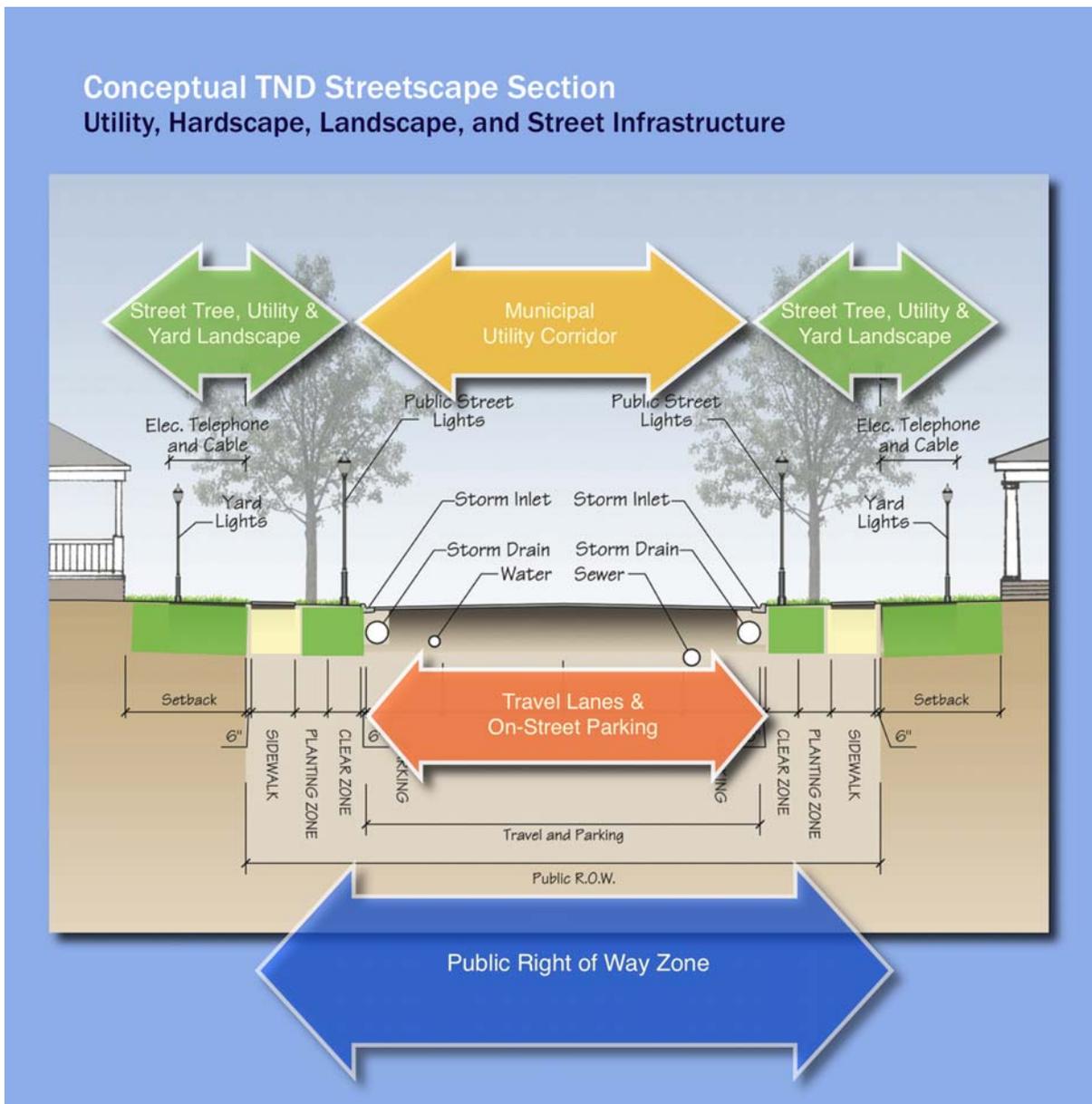
Where dictated by final design considerations, service easements may be located between the public rights of way and the individual building sites. Services shall be located in these “utility corridors” as may be required. All service connections shall be located underground and away from existing and proposed trees and special features. In the Town Center transects, exterior electric service conduits and connections shall be provided along designated streets to provide accessible receptacles for use during public events and occasional street festivals.

Within the Town Center, electric transformers and appurtenances should be adequately screened by wood or masonry walls, landscaping, or other materials that are complementary with the adjacent building, or placed in underground vaults. In addition to Town ordinance requirements, the placement, design, and construction of all transformers will comply with any additional criteria as may be imposed by the Design Review Committee. Transformers should not be placed in highly visible locations.

Waste storage and pick-up facilities for Town Center commercial and mixed-use sites shall be located in areas with minimum visibility from roadways and pedestrian circulation. When located on the building exterior, storage receptacles should be enclosed by masonry walls or other materials complementary with the adjacent building. The waste storage and pick-up area should be located with other service and facilities, preferably within alleys or in screened areas contiguous to Town Center parking areas. Other rules may be promulgated for residential sites subject to Town regulations and Design Review Committee input.

Utility and Public Streetscape Infrastructure

Within the TND, a range of public streets will be developed, each having its unique right of way, pavement, and streetscape characteristics. Fundamental to successful coordination between the TND public streets and urban infrastructure is a compact system of utility design for implementation within the public rights of way. This requires a level of engineering attention that reflects urban development practices while departing from traditional suburban utility design practices common to large lot subdivisions. The diagram below depicts the prototype utility schematic for the internal street system within the Round Hill TND .



Appendix A:

**Illustration: Calculation Procedures for
Minimum and Maximum Land Use Mix and Yield for
Residential and Non-Residential/Mixed Use Transects**



Round Hill/UNO-TND District

The Town of Orange

APPENDIX A: Transect Mix and Yield Illustrative Examples

Calculation Procedures for Minimum and Maximum Land Use Mix and Yield for Residential and Non-Residential/Mixed Use Transects

The Regulating Plan establishes six internal Transect Zones within the Round Hill/ Uptown North Orange TND. See *Section 4: Transect Map Diagram* and supporting maps which comprise the Regulating Plan exhibits as required by the TND District ordinance. The *Transect Zone Regulations* (see *Section 5*) establish the requirements for the minimum and maximum thresholds for (1) the densities and (2) the mix of uses permitted within each transect zone.

The following *Figures 2-5* provide an illustration of the calculation procedure to establish the permitted densities and mix of uses in each of the transect zones. In addition to minimum and maximum regulatory densities, sample calculations are provided for land use scenarios within each transect that examine just two of any infinite number of possible combinations of use permitted by the flexible standards established in the TND District. *Tables A.1* and *A.2* summarize the regulatory minimum and maximum densities and land use mix for each transect.

To support the transect calculations, the following steps provide the general methodology to determine the maximum permitted residential dwellings (**MXPRD**) permitted within a given transect zone:

Step 1:

Determine the total land area (**TGLA**) of the transect (see *Transect Map Diagram* for transect acreage).

Step 2:

Determine maximum permitted densities and mix for residential uses in transect.

See *Table A.1* for summary of minimum and maximum permitted density for residential (**MXPD** and **MNPD**) and non-residential/mixed uses (**MXPFAR** and **MNPFAR**), and *Table 2.1* for minimum and maximum of land use percentage (%) of total transect land area (**TGLA**) to which density may be applied.

Step 3:

Calculate the maximum qualifying land area to which maximum residential density may be applied:

TAR, transect area allocated to residential use =

[(Total Transect Gross Land Area, *in acres*) – (Transect Area Allocated to Non-Residential/Mixed-Use Areas, *in acres*)] or

TAR = (TGLA - TANR)

Step 4:

Determine the maximum number of residential dwellings permitted in the transect.

Maximum Permitted Residential Dwellings =

(Maximum Permitted Density in T5A Transect, *in dwelling units per acre*) X
[(Total Transect Gross Land Area, *in acres*) –
(Transect Area Allocated to Non-Residential/Mixed-Use Areas, *in acres*)] or:

MXPRD = MXPD X (TGLA - TANR)

where

TAR:	Maximum transect area allocated to residential use = (TGLA - TANR)
MXPRD:	Maximum permitted residential dwellings (<i>in dwelling units</i>)
MXPD:	Maximum permitted density in transect (<i>in dwelling units per acre</i>)
MNPRD:	Minimum permitted residential dwellings (<i>in dwelling units</i>)
MNPD:	Minimum permitted density in transect (<i>in dwelling units per acre</i>)
TGLA:	Transect gross land area (<i>in acres</i>)
TANR:	Transect area allocated to non-residential and mixed-use (<i>min., in acres</i>)

The same methodology can be applied to calculate the minimum and maximum residential yields as well as the minimum and maximum non-residential/mixed-use development thresholds in each transect. The following [Figures 2-5](#) demonstrate the application of this formula for each of the transects. The calculations present an absolute minimum and maximum yield for the two land use categories ([see Figure 6](#)) along with two hypothetical scenarios for each transect that yields that lie between the minimum and maximum allowances.

To achieve the maximum yield in the residential category, the absolute dwelling units count must be offset by the yield and area allocated to the non-residential/mixed-uses. [Figure 3](#) provides an example that focuses on the T5A Transect Zone: In the T5A Transect Zone (17.06 acres), if the residential area allocation reaches the stated maximum of 70%, it will yield 287 dwellings for 11.94 qualifying acres. Therefore, only 30% of the remaining qualifying land area can be allocated to the application of non-residential/mixed-use density, yielding the minimum allowable of 44,588 sfgfa on 5.12 qualifying acres.

For example, [Figure 3](#) indicates that the T5A Transect density allocation for residential cannot exceed a maximum of 70% while the non-residential/mixed-use allocation cannot exceed 100%. By establishing minimum density thresholds, the calculation procedure makes it clear that both the residential and non-residential maximum densities can be achieved at full development. In this case, the minimum non-residential density allocation is 30%, allowing for the transect to accommodate 70% residential.

On the other hand, if the density allocation flexibility provisions of the TND District are applied, the land use mix could comfortably rest between the minimum and maximum allowable ranges. In the sample presented in [Figure 3](#), Scenario 2 could potentially achieve a 40% qualifying residential transect area mix with commercial/mixed-use comprising the 60% balance. The application of the formula to this example would yield 109 residential dwellings and 222,940 sfgfa of non-residential building space in the T5A Transect Zone.

It is important to recognize (and keep track of) the flexibility offered by the Town's TND District regulations. It is unlikely that either the extreme minimum or maximum yields will ultimately be built. As the project develops over time, the code acknowledges that the mix and match of residential and non-residential uses will fluctuate between the regulatory minimums and maximums for each transect.

As development proceeds, each transect will have to be closely monitored to account for the "float" between its residential and non-residential allocations. The Code of Development offers a simple means of doing so: With each individual development site plan and subdivision proposal, the applicant will be required to document its mix and density allocation to the overall minimum and maximum thresholds. The process of maintaining a proper accounting of the actual cumulative mix and yield compliance requirements is addressed in the [Additional Transect Development Regulations](#) for each transect zone.

Table A.1: Round Hill / Uptown North Orange
Minimum & Maximum Density by Transect Land Area

TRANSECT	Transect Density Regulations		TRANSECT
ZONE	Residential (Dwelling Units per Acre)	Non-Residential / Mixed Use (Floor Area Ratio)	ZONE
T2	Not Permitted	Not Permitted	T2
T3	Minimum: 3.0 du/ac Maximum: 6.0 du/ac	Not Permitted	T3
T4	Minimum: 6.0 du/ac Maximum: 9.0 du/ac	Minimum: Not Regulated Maximum: 0.50 FAR	T4
T5A	Minimum: 12.0 du/ac Maximum: 24.0 du/ac	Minimum: 0.20 FAR Maximum: 1.0 FAR	T5A
T5B	Minimum: 9.0 du/ac Maximum: 18.0 du/ac	Minimum: 0.20 FAR Maximum: 0.75 FAR	T5B
SD15	Minimum: 12.0 du/ac Maximum: 24.0 du/ac	Minimum: 0.20 FAR Maximum: 1.0 FAR	SD15

Regulatory Notes:

1. This table is regulatory. See individual transect zone regulations for minimum and maximum allowable density. See Transect Map Diagram for applicable transect land area.
2. See Land Use Table for by-right and special permit uses.
2. See Table A.2 for land use mix requirements (in terms of percentage of qualifying land area for residential and non-residential/mixed-use areas.)

**Table A.2: Round Hill / Uptown North Orange
Land Use Mix Requirements by Qualifying Transect Land Area**

TRANSECT	Qualifying Transect Area by Land Use Mix		TRANSECT
ZONE	Residential	Non-Residential / Mixed Use	ZONE
T2	Minimum: 0% Maximum: 0%	Minimum: 0% Maximum: 0%	T2
T3	Minimum: 100% Maximum: 100%	Minimum: 0% Maximum: 0%	T3
T4	Minimum: 90% Maximum: 100%	Minimum: 0% Maximum: 10%	T4
T5A	Minimum: 0% Maximum: 70%	Minimum: 30% Maximum: 100%	T5A
T5B	Minimum: 20% Maximum: 80%	Minimum: 20% Maximum: 80%	T5B
SD15	Minimum: 0% Maximum: 50%	Minimum: 50% Maximum: 100%	SD15

Regulatory Notes:

1. This table is regulatory. See individual transect zone regulations for minimum and maximum allowable density. See Transect Map Diagram for applicable transect land area.
2. Maximum residential dwellings in transect = (Maximum allowable transect density) X (Total transect land area – Qualifying transect area allocated to non-residential and mixed-use development)
3. Maximum non-residential and mixed use floor area in transect = (Maximum allowable FAR) x (Total transect land area – Qualifying transect area allocated to residential uses)
4. The applicant shall provide a cumulative account of permitted residential and non-residential/mixed-use transect land area with the submission of subdivision plat and site plan.
5. See Land Use Table for by-right and special permit uses. Building floor areas for uses permitted in the T2 Transect Zone shall be regulated at time of application for site plan or special permit approval.

APPENDIX A: Figure 1

Round Hill TND: T3 Transect

Illustration of Land Use Mix and Yield

T4 Transect Zone Area: **31.93 ac.**

Regulatory:	Residential Density			Non-Residential/Mixed Use Floor Area Ratio		
	Minimum Density (du/ac)	Possible Density (du/ac)	Maximum Density (du/ac)	Minimum Density (FAR)	Possible Density (FAR)	Maximum Density (FAR)
	3	4	6	0	0	0

Regulatory Range Permitted: Absolute Minimum & Maximum for Residential and Non-Residential Transect Yield

Regulatory:	Minimum %	Possible %	Maximum %	Minimum %	Possible %	Maximum %
Transect Land Use Mix	100%		100%	0%		10%
Qualifying Transect Area	28.74 ac.		31.93 ac.	0.00 ac.		3.19 ac.
Yield (Range)	86 du		192 du	- sfgfa		- sfgfa

Scenario 1: Possible Mix of T3 Transect Land Uses

Transect Land Use Mix	100%	0%
Qualifying Transect Area	31.93 ac.	0.00 ac.
Yield (Range)	128 du	- sfgfa

Scenario 2: Possible Mix of T3 Transect Land Uses

Transect Land Use Mix	100%	0%
Qualifying Transect Area	31.93 ac.	0.0 ac.
Yield (Range)	128 du	- sfgfa

APPENDIX A: Figure 2

Round Hill TND: T4 Transect

Illustration of Land Use Mix and Yield

T4 Transect Zone Area: **22.87 ac.**

Regulatory:	Residential Density			Non-Residential/Mixed Use Floor Area Ratio		
	Minimum Density (du/ac)	Possible Density (du/ac)	Maximum Density (du/ac)	Minimum Density (FAR)	Possible Density (FAR)	Maximum Density (FAR)
	6	8	12	0	0.2	0.5

Regulatory Range Permitted: Absolute Minimum & Maximum for Residential and Non-Residential Transect Yield

Regulatory:	Residential		Non-Residential/Mixed Use	
	Minimum %	Maximum %	Minimum %	Maximum %
Transect Land Use Mix	90%	100%	0%	10%
Qualifying Transect Area	20.6 ac.	22.9 ac.	0.0 ac.	2.3 ac.
Yield (Range)	123 du	274 du	- sfgfa	49,811 sfgfa

Scenario 1: Possible Mix of T4 Transect Land Uses

Transect Land Use Mix	100%	0%
Qualifying Transect Area	22.87 ac.	0.00 ac.
Yield (Range)	183 du	- sfgfa

Scenario 2: Possible Mix of T4 Transect Land Uses

Transect Land Use Mix	95%	5%
Qualifying Transect Area	21.73 ac.	1.1 ac.
Yield (Range)	174 du	9,962 sfgfa

APPENDIX A: Figure 3

Round Hill TND: T5A Transect

Illustration of Land Use Mix and Yield

T5A Transect Zone Area: 17.06 ac.

Regulatory:	Residential Density			Non-Residential/Mixed Use Floor Area Ratio		
	Minimum Density (du/ac)	Possible Density (du/ac)	Maximum Density (du/ac)	Minimum Density (FAR)	Possible Density (FAR)	Maximum Density (FAR)
	12	16	24	0.2	0.5	1

Regulatory Range Permitted: Absolute Minimum & Maximum for Residential and Non-Residential Transect Yield

Regulatory:	Minimum %		Maximum %	Minimum %		Maximum %
Transect Land Use Mix	0%		70%	30%		100%
Qualifying Transect Area	0.00 ac.		11.94 ac.	5.12 ac.		17.06 ac.
Yield (Range)	0 du		287 du	44,588 sfgfa		743,134 sfgfa

Scenario 1: Possible Mix of T5A Transect Land Uses

Transect Land Use Mix	40%	60%
Qualifying Transect Area	6.82 ac.	10.24 ac.
Yield (Range)	109 du	222,940 sfgfa

Scenario 2: Possible Mix of T5A Transect Land Uses

Transect Land Use Mix	15%	85%
Qualifying Transect Area	2.56 ac.	14.50 ac.
Yield (Range)	41 du	315,832 sfgfa

APPENDIX A: Figure 4

Round Hill TND: T5B Transect

Illustration of Land Use Mix and Yield

T5B Transect Zone Area: 23.83 ac.

Regulatory:	Residential Density			Non-Residential/Mixed Use Floor Area Ratio		
	Minimum Density (du/ac)	Possible Density (du/ac)	Maximum Density (du/ac)	Minimum Density (FAR)	Possible Density (FAR)	Maximum Density (FAR)
	9	12	18	0.2	0.3	0.75

Regulatory Range Permitted: Absolute Minimum & Maximum for Residential and Non-Residential Transect Yield

Regulatory:	Minimum %		Maximum %	Minimum %		Maximum %
Transect Land Use Mix	20%		80%	20%		80%
Qualifying Transect Area	4.77 ac.		19.06 ac.	4.77 ac.		19.06 ac.
Yield (Range)	43 du		343 du	41,521 sfgfa		622,821 sfgfa

Scenario 1: Possible Mix of T5B Transect Land Uses

Transect Land Use Mix	40%	60%
Qualifying Transect Area	9.53 ac.	14.30 ac.
Yield (Range)	114 du	186,846 sfgfa

Scenario 2: Possible Mix of T5B Transect Land Uses

Transect Land Use Mix	50%	50%
Qualifying Transect Area	11.92 ac.	11.92 ac.
Yield (Range)	143 du	155,705 sfgfa

APPENDIX A: Figure 5

Round Hill TND: SD15 Transect

Illustration of Land Use Mix and Yield

T5B Transect Zone Area (TGLA): **4.40 ac.**

Regulatory:	Residential Density			Non-Residential/Mixed Use Floor Area Ratio		
	Minimum Density (du/ac)	Possible Density (du/ac)	Maximum Density (du/ac)	Minimum Density (FAR)	Possible Density (FAR)	Maximum Density (FAR)
	12	12	24	0.2	0.25	1

Regulatory Range Permitted: Absolute Minimum & Maximum for Residential and Non-Residential Transect Yield

Regulatory:	Minimum %	Possible %	Maximum %	Minimum %	Possible %	Maximum %
Transect Land Use Mix	0%		50%	50%		100%
Qualifying Transect Area	0.00 ac.		2.20 ac.	2.20 ac.		4.40 ac.
Yield (Range)	0 du		53 du	19,166 sfgfa		191,664 sfgfa

Scenario 1: Possible Mix of SD15 Transect Land Uses

Transect Land Use Mix	0%	100%
Qualifying Transect Area	0.00 ac.	4.40 ac.
Yield (Range)	0 du	47,916 sfgfa

Scenario 2: Possible Mix of SD15 Transect Land Uses

Transect Land Use Mix	50%	50%
Qualifying Transect Area	2.20 ac.	2.20 ac.
Yield (Range)	26 du	23,958 sfgfa

APPENDIX A: Figure 6

Round Hill TND

Summary of Transect Zone Land Use Yield Potential

Transect	Area (ac.)	Residential		Non-Residential	
		Minimum (du)	Maximum (du)	Minimum (sfgfa)	Maximum (sfgfa)
T2	34.94	<i>na</i>	<i>na</i>	<i>na</i>	<i>na</i>
T3	31.93	86	192	-	-
T4	22.87	123	274	-	49,811
T5A	17.06	0	287	44,588	743,134
T5B	23.83	43	343	41,521	622,821
SD15	4.4	0	53	19,166	239,580

Notes:

1. Yield calculations represent absolute minimum and maximum allowable for residential and non-residential uses. Actual yield within each transect will be less than the combined maximum residential and non-residential yield calculations. See tables herein that provide examples of possible land use mix and yield combinations for individual transects.
2. See *Transect Map Diagram* for transect zone land areas per revised Code of Development.
3. See *Table A.2* for land use mix requirements.
4. See individual transect zone regulations for minimum and maximum residential densities and floor area ratios.